

THIS DOCUMENT HAS BEEN DIGITALLY SIGNED AND SEALED BY:  
**Tiffany Reid** Digitally signed by Tiffany Reid  
 Date: 2026.04.07 14:35:23 -05'00'

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PAPE-DAWSON  
 315 WOODLAND STREET  
 NASHVILLE, TN  
 TIFFANY REID, PE NO. 120979

THE ABOVE NAMED PROFESSIONAL ENGINEER SHALL BE RESPONSIBLE FOR THE FOLLOWING SHEETS IN ACCORDANCE OF TENN. CODE ANN. §62-2-306.

SHEET NAME	SHEET NO.
SIGNATURE SHEETS .....	ROADWAY-SIGN1
TITLE SHEET .....	1
ROADWAY INDEX, STANDARD ROADWAY DRAWINGS AND STANDARD TRAFFIC DESIGN DRAWINGS.....	1A
PROJECT COMMITMENTS.....	1B
ESTIMATED ROADWAY QUANTITIES .....	2
TYPICAL SECTIONS AND PAVEMENT SCHEDULE .....	2B, 2B1
GENERAL NOTES.....	2C
SPECIAL NOTES.....	2D
ENVIRONMENTAL NOTES.....	2E
TABULATED QUANTITIES .....	2F, 2F1
DETAIL SHEETS.....	2G, 2G1-2G4
UTILITY NOTES AND UTILITY OWNERS.....	3
PAVEMENT EDGE DROP-OFF NOTES FOR TRAFFIC CONTROL.....	T1

YEAR	PROJECT NO.	SHEET NO.
2026	STP/HSP-2(304)	ROADWAY-SIGN1

**STATE OF TENNESSEE  
 DEPARTMENT OF TRANSPORTATION**

**SIGNATURE  
 SHEET**

Index Of Sheets  
SEE SHEET NO. 1A

# STATE OF TENNESSEE DEPARTMENT OF TRANSPORTATION BUREAU OF ENGINEERING

DOES THIS PROJECT QUALIFY FOR UTILITY CHAPTER 86	YES	NO X
WORK ZONE SIGNIFICANCE DETERMINATION		
SIGNIFICANT	YES	NO X
RAILROAD INVOLVEMENT	YES	NO X

TENN.	YEAR	SHEET NO.
	2026	1
FED. AID PROJ. NO.	STP/HSIP-2(304)	
STATE PROJ. NO.	58S002-F3-007, 58S002-F8-007	
BRIDGE NO.	58S002-M3-007	

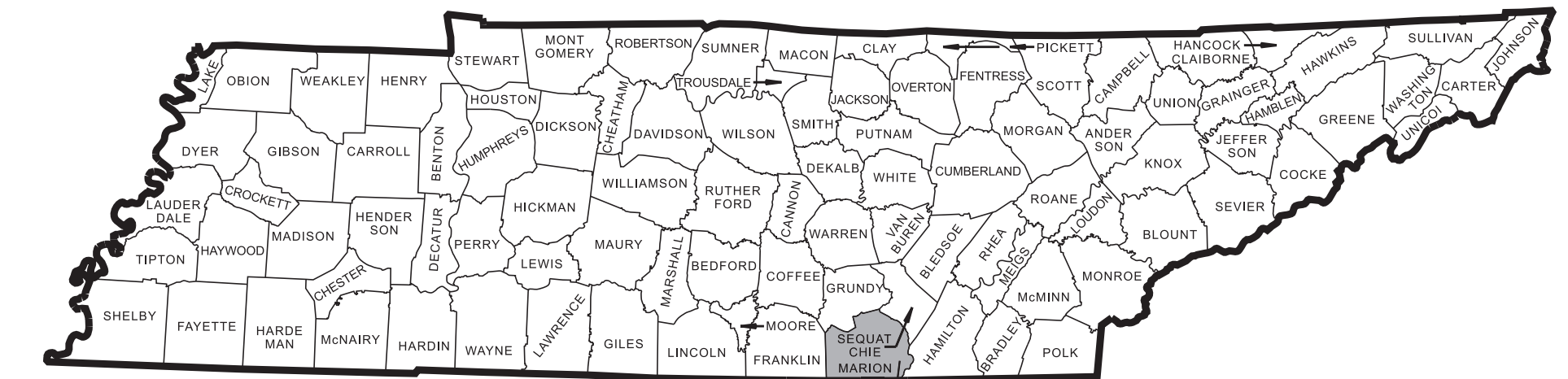
PROJECT TO BE LET WITH:  
MARION CO. SR-15  
PIN 132857.00

## MARION COUNTY

STATE ROUTE 2  
FROM ASSEMBLY AVENUE  
TO NORTH OF WELLS ROAD WITHIN MONTEAGLE

### RESURFACE & SAFETY

MILL, CS, 411D, INTELLIGENT COMPACTION, PAVEMENT MARKINGS, AND CURB RAMPS  
STATE HIGHWAY NO. 2 F.A.H.S. NO. N/A



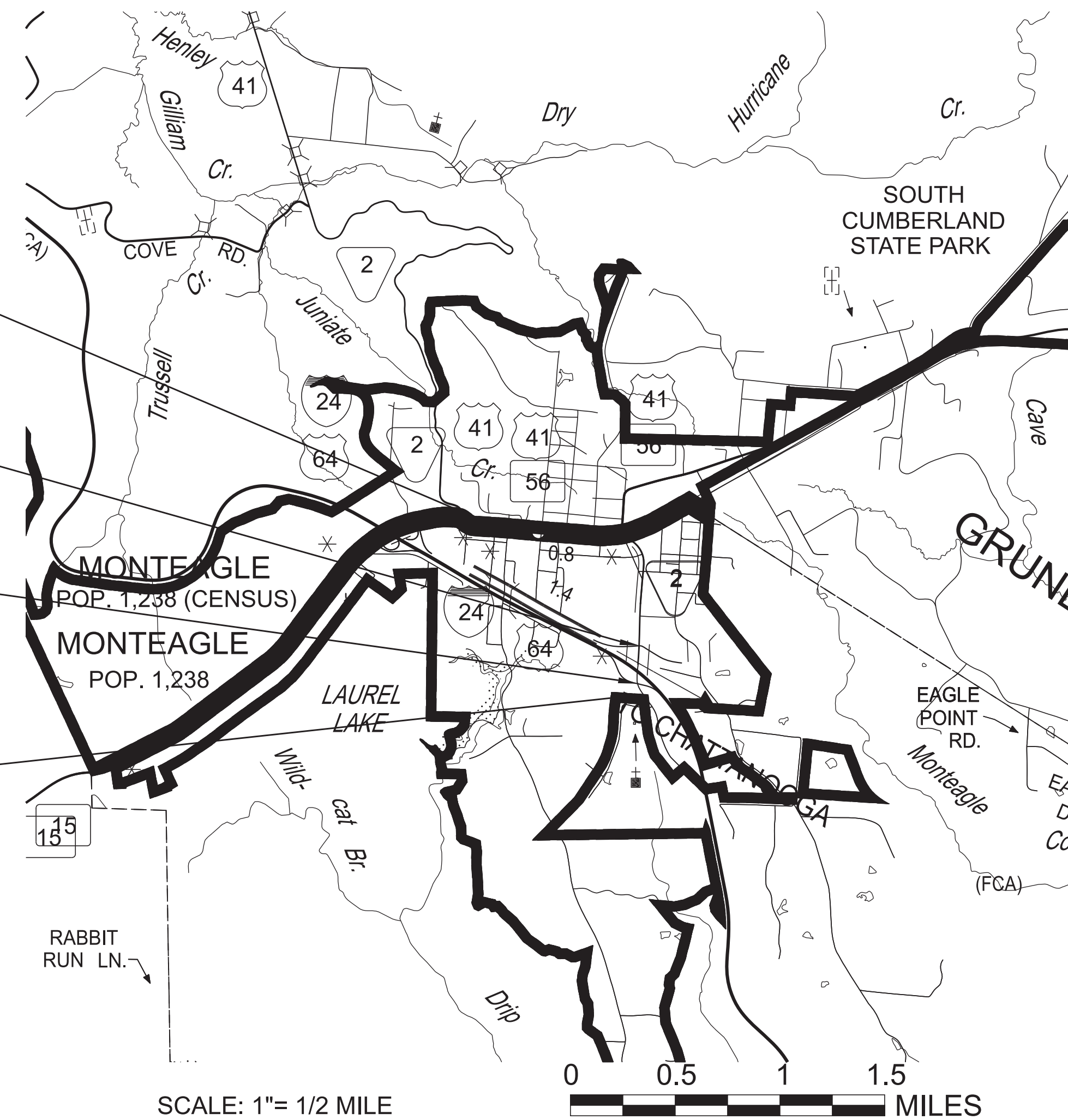
PROJECT LOCATION  
BRIDGE ID. # 58I00240071, 58I00240007

58S002-F3-007  
58S002-F8-007  
BEGIN PROJECT NO. STP/HSIP-2(304) RESURFACE & SAFETY  
L.M. 0.231

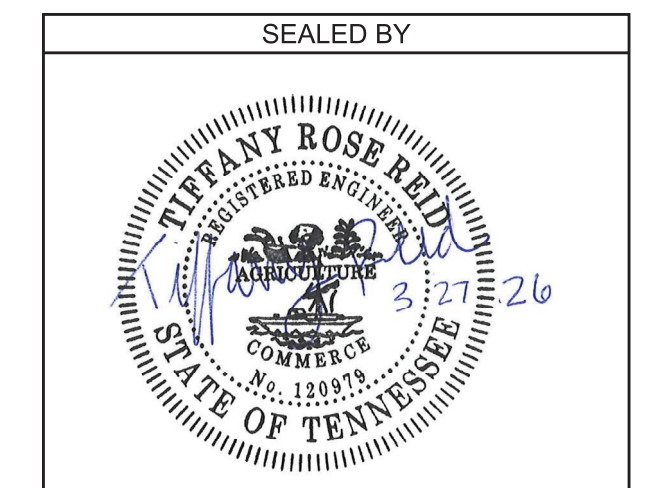
I-24 WB CROSSING BRIDGE REPAIR PROJECT NO. 58S002-M3-007  
L.M. 1.40


I-24 EB CROSSING BRIDGE REPAIR PROJECT NO. 58S002-M3-007  
L.M. 0.011

58S002-F3-007  
58S002-F8-007  
END PROJECT NO. STP/HSIP-2(304) RESURFACE & SAFETY  
L.M. 0.13 (DIXIE LEE AVENUE)




NO EXCLUSIONS



APPROVED:   
SHANE HESTER, PE CHIEF ENGINEER

DATE: \_\_\_\_\_

APPROVED:   
WILL REID, PE COMMISSIONER

TRAFFIC COUNTER AND WEATHER STATIONS	
STATION LOCATION	LOG MILE
TRAFFIC COUNT STATION 86	0.292

TRAFFIC DATA	
ADT (2026)	8,990
POSTED SPEED	35 MPH

SPECIAL NOTES

PROPOSALS MAY BE REJECTED BY THE COMMISSIONER IF ANY OF THE UNIT PRICES CONTAINED THEREIN ARE OBVIOUSLY UNBALANCED, EITHER EXCESSIVE OR BELOW THE REASONABLE COST ANALYSIS VALUE.

THIS PROJECT TO BE CONSTRUCTED UNDER THE STANDARD SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION DATED APRIL 1, 2026 AND ADDITIONAL SPECIFICATIONS AND SPECIAL PROVISIONS CONTAINED IN THE PLANS AND IN THE PROPOSAL CONTRACT.

PROJECT LENGTH 1.339 MILES  
TOTAL LANE MILES RESURFACED 3.89 MILES

TDOT PROJECT MANAGER: ALLEN PETERFREUND, PE  
DESIGN FIRM : PAPE-DAWSON  
DESIGNER : KADE BOENEKE CHECKED BY : TIFFANY REID, PE  
P.E. NO. 98023-4217-04 (DESIGN)  
PIN NO. 134114.00

# ROADWAY INDEX

SHEET NAME	SHEET NO.
SIGNATURE SHEETS.....	ROADWAY-SIGN1
TITLE SHEET .....	1
ROADWAY INDEX, STANDARD ROADWAY DRAWINGS, AND STANDARD TRAFFIC DESIGN DRAWINGS .....	1A
PROJECT COMMITMENTS.....	1B
ESTIMATED ROADWAY QUANTITIES .....	2
TYPICAL SECTIONS AND PAVEMENT SCHEDULE .....	2B, 2B1
GENERAL NOTES.....	2C
SPECIAL NOTES.....	2D
ENVIRONMENTAL NOTES.....	2E
TABULATED QUANTITIES .....	2F, 2F1
DETAIL SHEETS.....	2G, 2G1-2G4
UTILITY NOTES, AND UTILITY OWNERS.....	3
PAVEMENT EDGE DROP-OFF NOTES FOR TRAFFIC CONTROL .....	T1
BRIDGE PLANS.....	B-1
NOTES:	
THE ALPHABETICAL LETTERS "I", "O" AND "Q" ARE NOT USED IN THE NUMBERING OF SHEETS.	
NO SHEET 2A SERIES INCLUDED IN THIS PLAN SET.	
NO UTILITY SHEETS ARE INCLUDED IN THIS PLAN SET.	

# STANDARD ROADWAY DRAWINGS

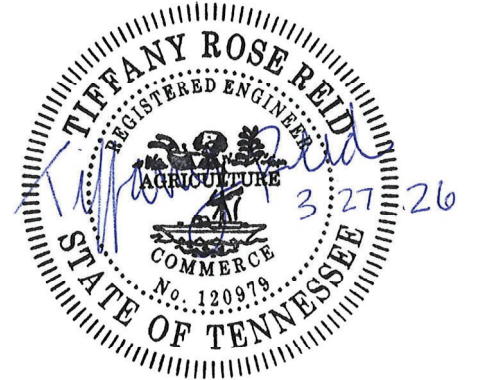
DWG.	REV.	DESCRIPTION
<b>10-100.00 STANDARD ROADWAY TITLE SHEET, ABBREVIATIONS, AND LEGENDS</b>		
RD-TP-1	09-26-16	STANDARD ROADWAY DRAWINGS TITLE SHEET
RD-A-1	02-20-20	STANDARD ABBREVIATIONS A THROUGH L
RD-A-2		STANDARD ABBREVIATIONS M THROUGH Z
RD-L-1	01-30-26	STANDARD LEGEND
RD-L-1A		STANDARD LEGEND
<b>10-107.00 MULTIMODAL</b>		
CR-GN-1		GENERAL NOTES CURB RAMP
CR-DWS-1		DETECTABLE WARNING SURFACE DETAIL
CR-CRT-1		CURB RAMP TYPES
CR-PL-2		PARALLEL CURB RAMP PLACEMENT LAYOUT
CR-20	07-01-25	PARALLEL CURB RAMP 5 – FT SIDEWALK
CR-33		SINGLE CROSSING CURB RAMP IN CURVE
CR-34		SINGLE CROSSING CURB RAMP IN CURVE
CR-40		MONO-DIRECTIONAL SINGLE CROSSWALK CURB RAMP DETAILS
CR-50		PEDESTRIAN REFUGE
CR-51		PEDESTRIAN REFUGE
MM-PM-1		SIGNING AND PAVEMENT MARKINGS AT INTERSECTION CROSSINGS FOR SHARED-USE PATHS
MM-TS-1	06-15-21	BIKE ACCOMMODATION DESIGN GUIDANCE

# STANDARD TRAFFIC DESIGN DRAWINGS

DWG.	REV.	DESCRIPTION
<b>10-204.00 DESIGN - TRAFFIC CONTROL</b>		
T-M-1	01-24-25	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS AND MARKING ABBREVIATIONS
T-M-2	01-24-25	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS
T-M-3	01-24-25	MARKING STANDARDS FOR TRAFFIC ISLANDS, PAVED SHOULDERS AND MEDIANS FOR CONVENTIONAL ROADS
T-M-4	01-24-25	STANDARD INTERSECTION PAVEMENT MARKINGS
T-M-4A	01-24-25	STANDARD UNSIGNALIZED MID-BLOCK CROSSING
<b>10-204.02 WORK ZONES</b>		
T-WZ-10	03-26-25	ADVANCE ROAD WORK SIGNING ON HIGHWAYS AND FREEWAYS
T-WZ-22		ONE LANE CLOSURE DETAIL FOR UNDIVIDED HIGHWAYS
T-WZ-60	03-26-25	FREEWAY RESURFACING SIGNING LAYOUT
T-WZ-FAB1	03-26-25	FLASHING YELLOW ARROW BOARD

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2026	STP/HSIP-2(304)	1A

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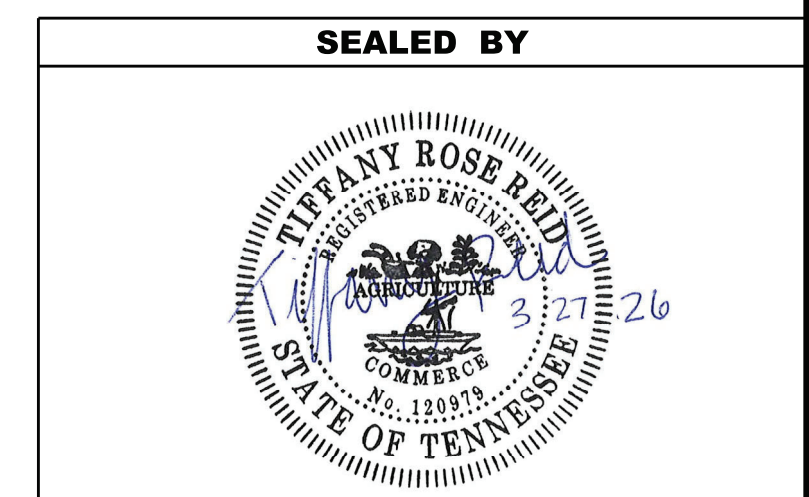


STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

ROADWAY INDEX,  
STANDARD ROADWAY  
DRAWINGS, AND STANDARD  
TRAFFIC DESIGN DRAWINGS

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2026	STP/HSIP-2(304)	1B

PROJECT COMMITMENTS			
COMMITMENT ID	SOURCE DIVISION	DESCRIPTION	STATION / LOCATION
EDHZ001	ENVIRONMENTAL DIVISION, HAZARDOUS MATERIALS	AN ASBESTOS CONTAINING MATERIAL (ACM) SURVEYS WERE COMPLETED ON BRIDGE NO. 58I00240071 DIXIE LEE AVE OVER I-24 WB LM 1.40 (58-SR002-01.40). NO ASBESTOS WAS DETECTED. PLEASE SEE THE REPORTS FOR FURTHER DETAILS AND PHOTOGRAPHS. NO SPECIAL ACCOMMODATIONS FOR DEMOLITION AND WASTE DISPOSAL ARE ANTICIPATED FOR THESE STRUCTURES AND THE MATERIAL CAN BE DEPOSITED IN A C&D LANDFILL. PRIOR TO THE DEMOLITION OR REHABILITATION OF ANY STRUCTURE (BRIDGE OR BUILDING), THE CONTRACTOR IS REQUIRED TO SUBMIT THE NATIONAL EMISSION STANDARDS FOR HAZARDOUS AIR POLLUTANTS STANDARD 10-DAY NOTICE OF DEMOLITION TO THE TDEC DIVISION OF AIR POLLUTION CONTROL (PER TDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION (JANUARY 1, 2021) SECTIONS 107.08.D AND 202.03).	BRIDGE NO. 58I00240071 DIXIE LEE AVE OVER I-24 WB LM 1.40
EDHZ002	ENVIRONMENTAL DIVISION, HAZARDOUS MATERIALS	AN ASBESTOS CONTAINING MATERIAL (ACM) SURVEY WAS COMPLETED ON BRIDGE NO. 58I00240007 SR-2 OVER I-24 EB LANES LM 0.01 (58-0A550-00.01). NO ASBESTOS WAS DETECTED. PLEASE SEE THE REPORT FOR FURTHER DETAILS AND PHOTOGRAPHS. NO SPECIAL ACCOMODATIONS FOR DEMOLITION AND WASTE DISPOSAL ARE ANTICIPATED FOR THESE STRUCTURES AND THE MATERIAL CAN BE DEPOSITED IN A C&D LANDFILL. PRIOR TO THE DEMOLITION OR REHABILITATION OF ANY STRUCTURE (BRIDGE OR BUILDING), THE CONTRACTOR IS REQUIRED TO THE NATIONAL EMISSION STANDARDS FOR HAZARDOUS AIR POLLUTANTS STANDARD 10-DAY NOTICE OF DEMOLITION TO THE TDEC DIVISION OF AIR POLLUTION CONTROL (PER TDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION (JANUARY 1, 2021) SECTIONS 107.08.D AND 202.03)	BRIDGE NO. 58I00240007 SR-2 OVER I-24 EB LANES LM 0.01



**STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION**

PROJECT  
COMMITMENTS

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2026	STP/HSIP-2(304)	2

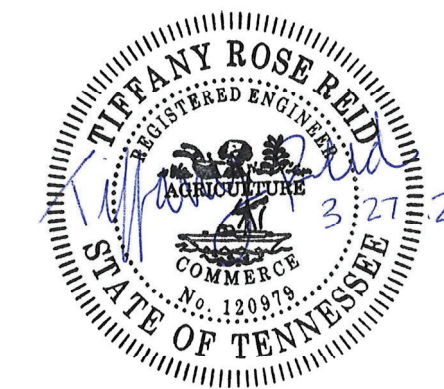
ESTIMATED ROADWAY QUANTITIES					
ITEM NO.	DESCRIPTION	UNIT	QUANTITY	QUANTITY	TOTAL QUANTITY
			58S002-F8-007	58S002-F3-007	
(1)	208-01.05 BROOMING & DEGRASSING SHOULDERS	L.M.	1.3		1.3
(2)	307-01.15 ASC MIX (PG64-22) (BPMLC-HM) GRADING CS	TON	983		983
	307-03.08 ASPHALT CONCRETE MIX (PG76-22) (BPMB-HM) GRADING B-M2	TON	150		150
(3)	403-02.01 TRACKLESS TACK COAT	TON	23		23
(4)	411-02.10 ACS MIX(PG70-22) GRADING D	TON	2605		2605
(5)	411-01.21 LONGITUDINAL JOINT SEALANT	L.M.	5		5
(6)	415-01.01 COLD PLANING BITUMINOUS PAVEMENT	TON	3478		3478
(7)	611-09.01 ADJUSTMENT OF EXISTING CATCHBASIN	EACH		8	8
	701-01.01 CONCRETE SIDEWALK (4 ")	S.F.		100	100
	701-02.01 CONCRETE CURB RAMP (RETROFIT)	S.F.		600	600
	701-02.03 CONCRETE CURB RAMP	S.F.		45	45
	712-01 TRAFFIC CONTROL	LS	0.5		0.5
	712-04.01 FLEXIBLE DRUMS (CHANNELIZING)	EACH	50		50
(8)	712-06 SIGNS (CONSTRUCTION)	S.F.	489		489
	712-08.03 ARROW BOARD (TYPE C)	EACH	1		1
(9)	713-16.01 CHANGEABLE MESSAGE SIGN UNIT	EACH	1		1
	716-01.21 SNOWPLOWABLE RAISED PAVEMENT MARKERS (BI-DIR) (1 COLOR)	EACH		185	185
(10)	716-02.03 PLASTIC PAVEMENT MARKING (CROSS-WALK)	L.F.	204		204
(10)	716-02.04 PLASTIC PAVEMENT MARKING(CHANNELIZATION STRIPING)	S.Y.		52	52
(10)	716-02.05 PLASTIC PAVEMENT MARKING (STOP LINE)	L.F.	412		412
(10)	716-02.06 PLASTIC PAVEMENT MARKING (TURN LANE ARROW)	EACH	1	10	11
(10)	716-04.12 PLASTIC PAVEMENT MARKING (YIELD LINE)	S.F.		40	40
(10)	716-04.13 PLASTIC PAVEMENT MARKING (BIKELANE SYMBOL & ARROW)	EACH	15		15
	716-05.01 PAINTED PAVEMENT MARKING (4" LINE)	L.M.	12.96		12.96
(11)	716-12.02 ENHANCED FLATLINE THERMO PVMT MRKNG (6IN LINE)	L.M.	6.48	2.5	8.98
	717-01 MOBILIZATION	LS	0.5		0.5

THERE ARE NO GUARDRAIL ADJUSTMENTS WITHIN THE PROJECT LIMITS.

### FOOTNOTES

- (1) INCLUDES THE COSTS OF REMOVING DEBRIS AND SWEEPING SHOULDERS PRIOR TO WORK. SEE SHEET NO. 2C NOTE 6 FOR MORE INFORMATION.
- (2) ITEM TO INCLUDE 983 TONS FOR THE MAINLINE.
- (3) ITEM TO INCLUDE 18 TONS FOR THE MAINLINE AND 5 TONS FOR THE SHOULDERS.
- (4) ITEM TO INCLUDE 1813 TONS FOR MAINLINE AND 792 TONS FOR SHOULDERS.
- (5) USE CRAFCO PAVEMENT JOINT ADHESIVE #34524 OR PAVON JOINT ADHESIVE BY PAVON CORPORATION.
- (6) ITEM TO INCLUDE 2694 TONS FOR MAINLINE AND 784 TONS FOR SHOULDERS.
- (7) ITEM TO BE USED FOR ADJUSTING EXISTING CATCH BASIN GRATES TO BE BICYCLE FRIENDLY.
- (8) SEE SHEET 2F FOR TABULATED QUANTITIES.
- (9) TO BE USED AS DIRECTED BY THE TDOT PROJECT ENGINEER.
- (10) CONTRACTOR MAY ELECT TO SUBSTITUTE PREFORMED PLASTIC FOR THERMOPLASTIC. PREFORMED PLASTIC SHALL BE PAID FOR AT THE SAME UNIT PRICE AS BID FOR THERMOPLASTIC.
- (11) CONTRACTOR SHALL USE THE EXTRUDED OR RIBBON METHOD FOR APPLICATION.

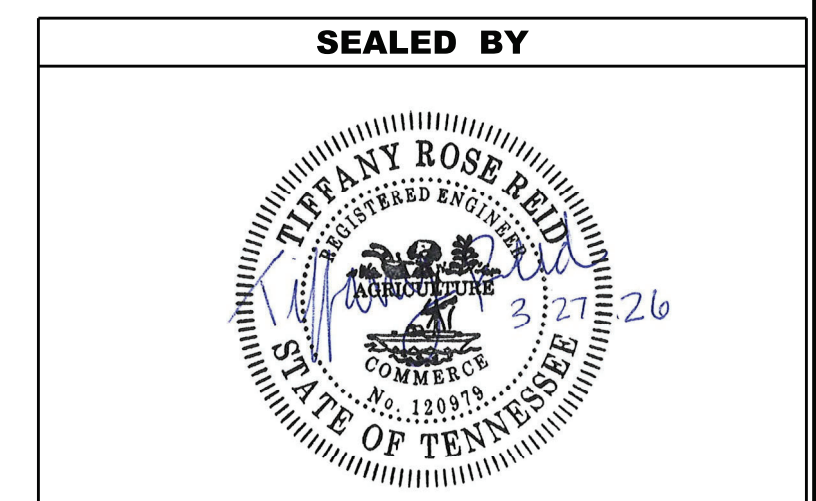
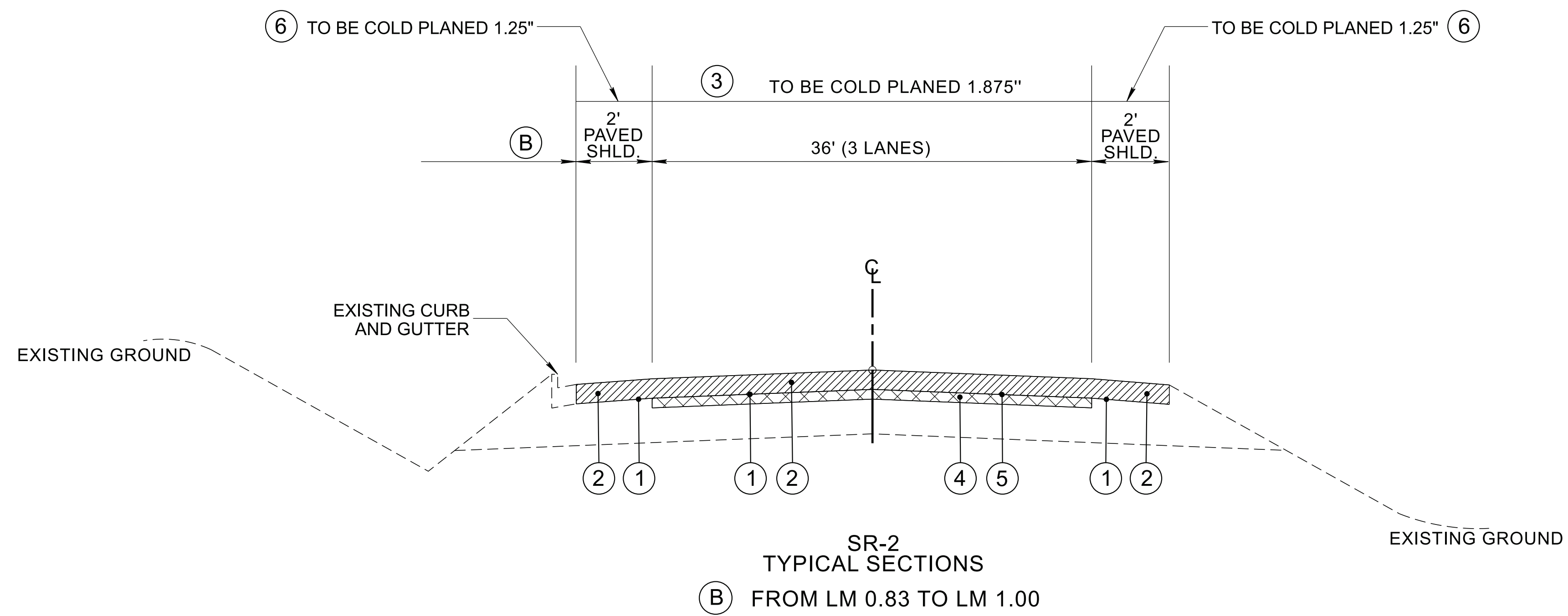
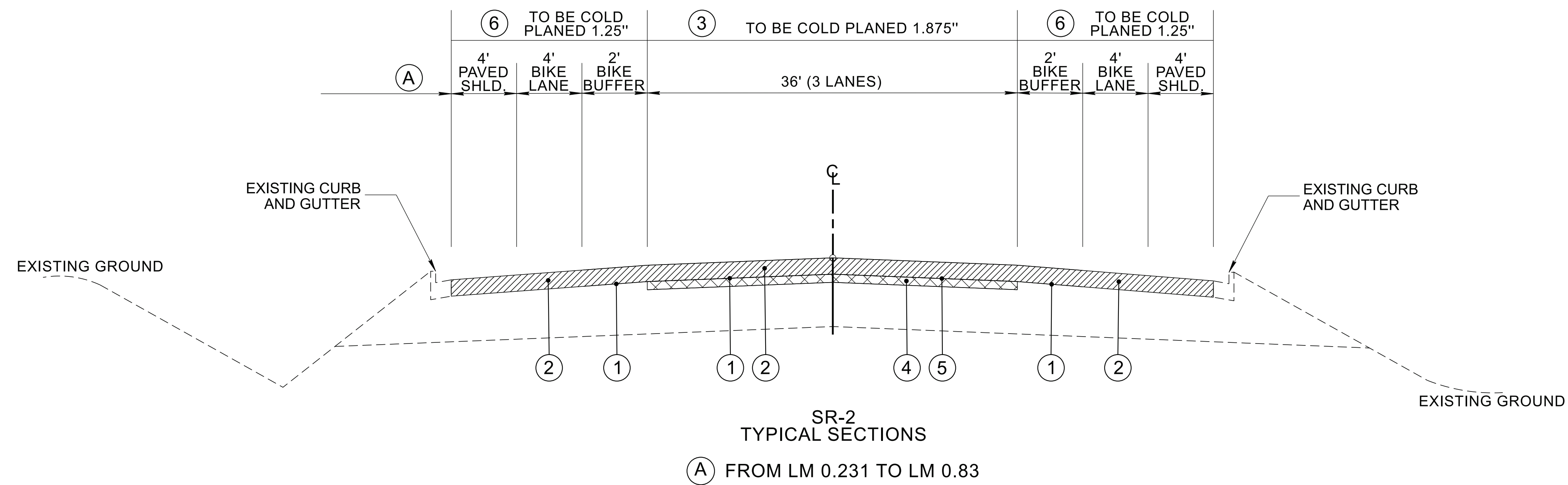
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STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

ESTIMATED  
ROADWAY  
QUANTITIES

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2026	STP/HSIP-2(304)	2B

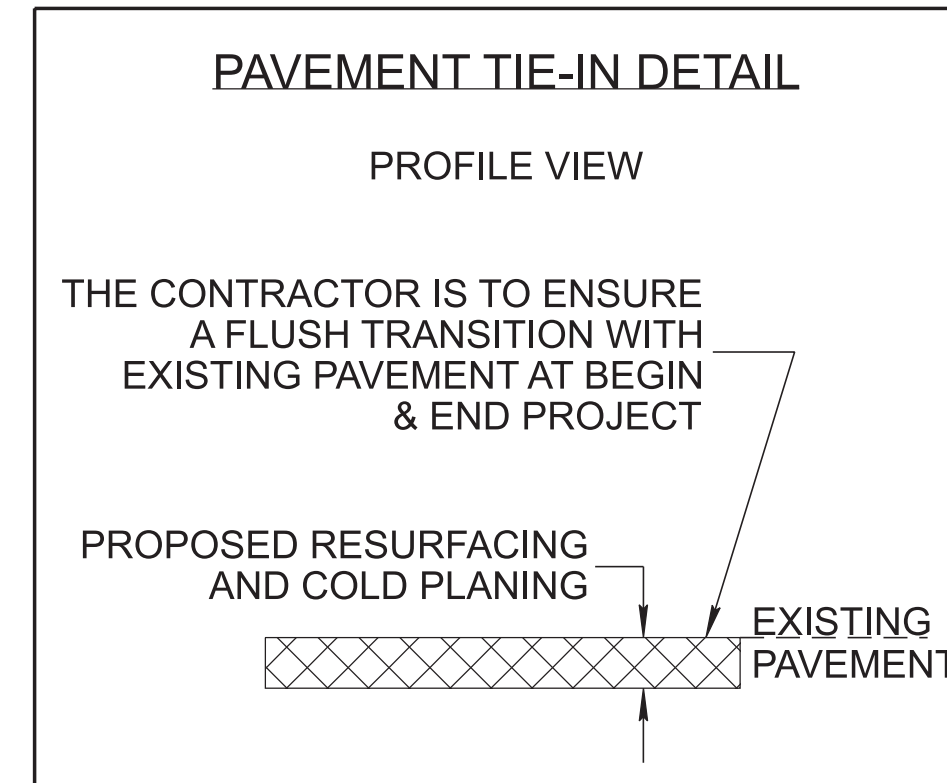
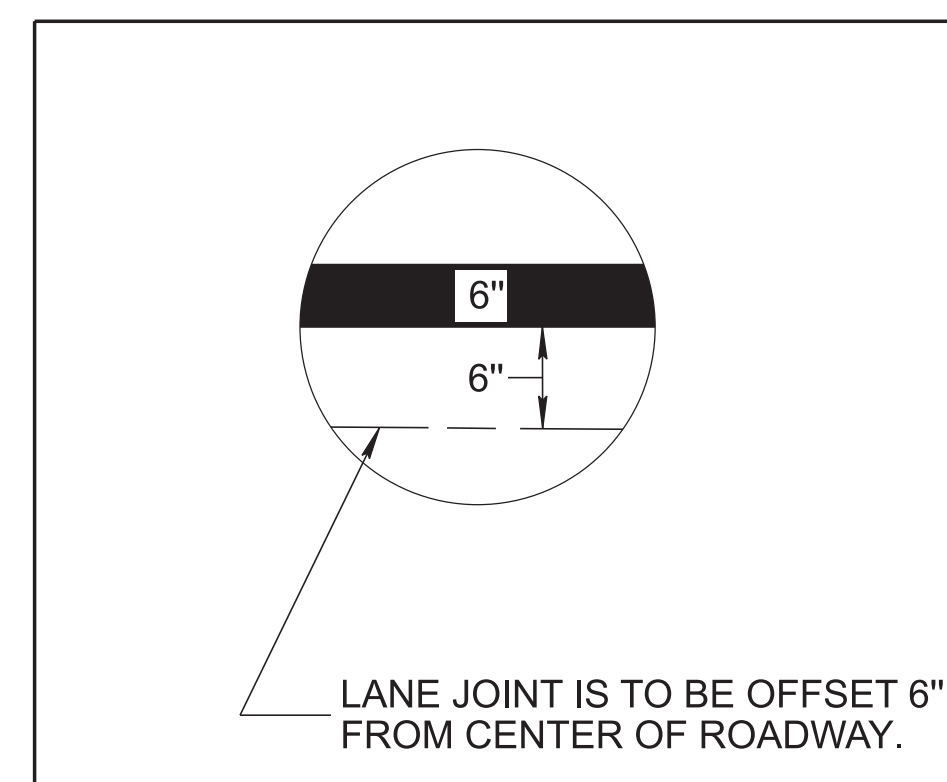
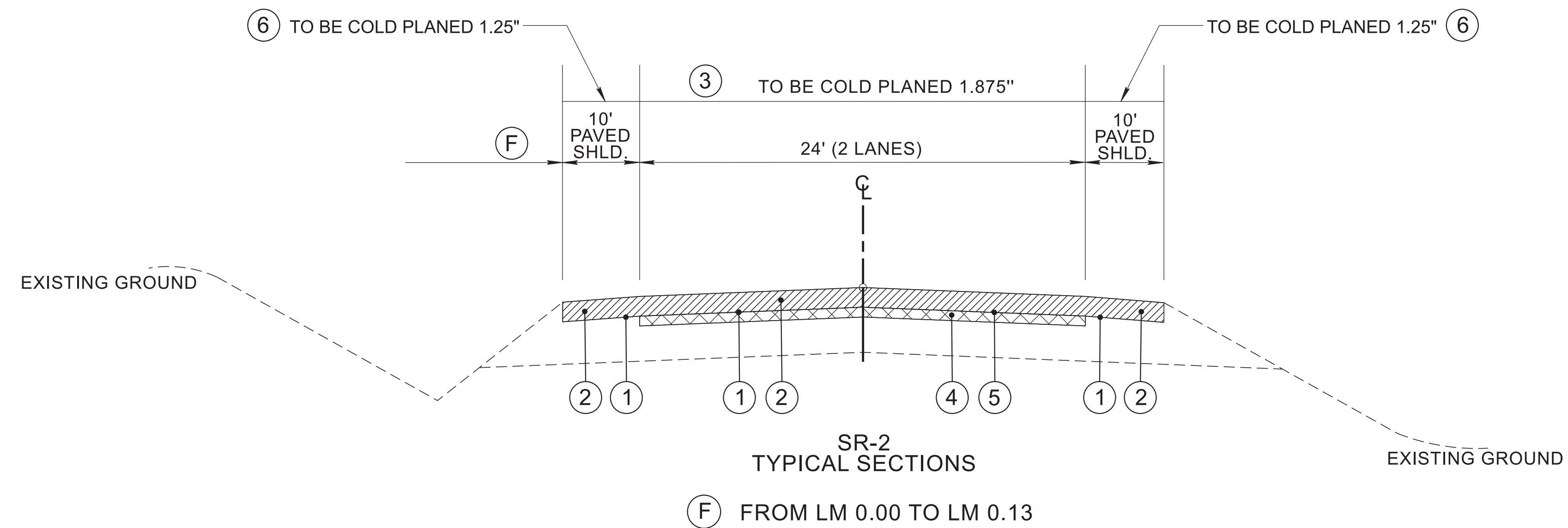
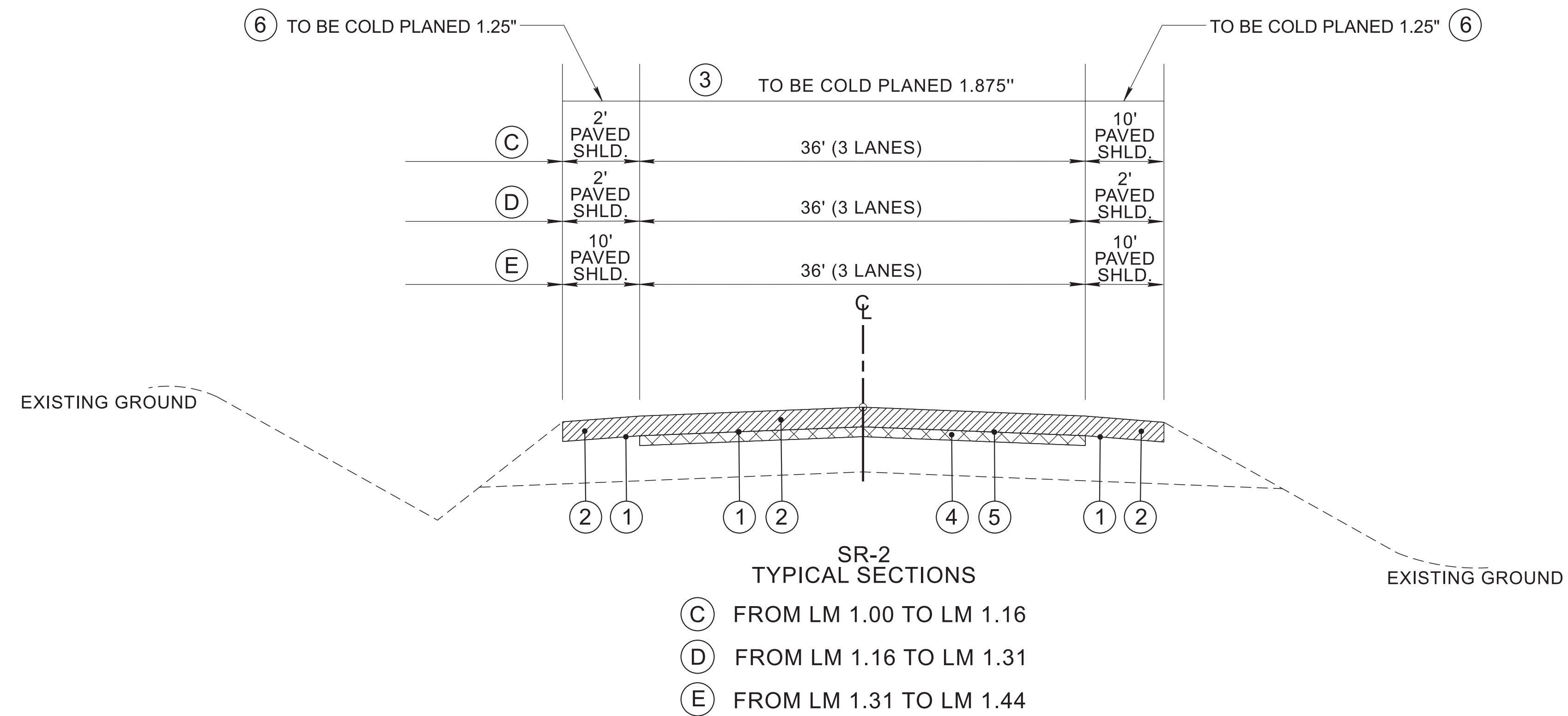


NOT TO SCALE

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS  
AND  
PAVEMENT SCHEDULE

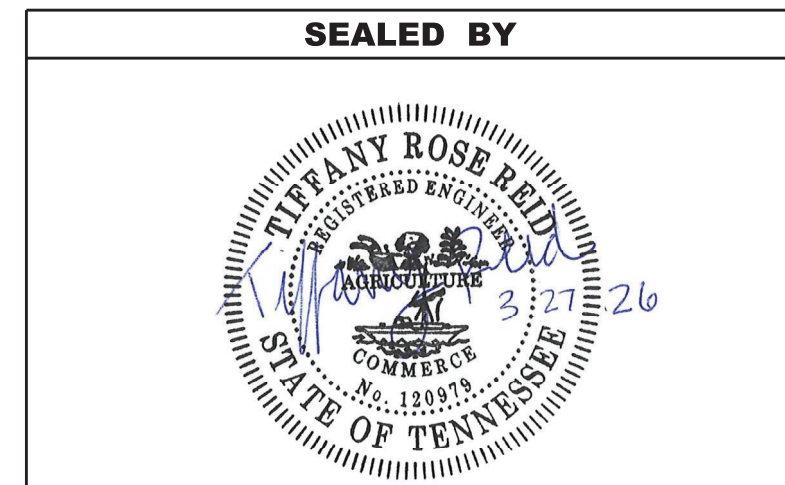
TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2026	STP/HSIP-2(304)	2B1



LONGITUDINAL JOINT SEALANT (ITEM 411-01.21) SHALL BE USED ON LANE JOINTS

PROPOSED PAVEMENT SCHEDULE	
(1) TRACKLESS TACK COAT (TC) @ 0.10 GAL/SY ITEM NO. 403-02.01 TRACKLESS TACK COAT (TC), TON	(4) ASPHALTIC BASE SURFACE (HM) 0.625" THICK (APPROX. 71.875 LB/SY) ITEM NO. 307-01.15 BASE MIX (PG64-22) GRADING CS
(2) ASPHALTIC CONCRETE SURFACE (HM) 1.25" THICK (APPROX. 132.5 LB/SY) ITEM NO. 411-02.10 ACS MIX (PG70-22) GRADING D	(5) TRACKLESS TACK COAT (TC) @ 0.05 GAL/SY ITEM NO. 403-02.01 TRACKLESS TACK COAT (TC), TON
(3) COLD PLANING 1.875" THICK (APPROX. 196.88 LB/SY) ITEM NO. 415-01.01 COLD PLANING BITUMINOUS MATERIAL, TON	(6) COLD PLANING 1.25" THICK (APPROX. 131.25 LB/SY) ITEM NO. 415-01.01 COLD PLANING BITUMINOUS MATERIAL, TON

NOTE: CONCRETE INTERSECTIONS AND/OR SIDEWALKS ARE NOT TO BE PAVED. THE CONTRACTOR IS RESPONSIBLE FOR KEEPING THEM CLEAN FOR THE DURATION OF THE PROJECT



NOT TO SCALE

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS  
AND  
PAVEMENT SCHEDULE

# GENERAL NOTES

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2026	STP/HSIP-2(304)	2C

## GRADING

- (1) ANY AREA THAT IS DISTURBED OUTSIDE LIMITS OF CONSTRUCTION DURING THE LIFE OF THIS PROJECT SHALL BE REPAIRED BY THE CONTRACTOR AT HIS EXPENSE.
- (3) THE CONTRACTOR SHALL NOT DISPOSE OF ANY MATERIAL EITHER ON OR OFF STATE-OWNED R.O.W. IN A REGULATORY FLOOD WAY AS DEFINED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY (FEMA) WITHOUT APPROVAL BY FEMA. ALL MATERIAL SHALL BE DISPOSED OF IN UPLAND (NON-WETLAND) AREAS AND ABOVE ORDINARY HIGH WATER OF ANY ADJACENT WATERCOURSE. THIS DOES NOT ELIMINATE THE NEED TO OBTAIN ANY OTHER LICENSES OR PERMITS THAT MAY BE REQUIRED BY ANY OTHER FEDERAL, STATE OR LOCAL AGENCY.

## MISCELLANEOUS

- (2) THE CONTRACTOR SHALL BE REQUIRED TO REMOVE AND RESET MAILBOXES AND POSTS WHERE AND AS DIRECTED BY THE ENGINEER. COST TO BE INCLUDED IN PRICE BID FOR OTHER CONSTRUCTION ITEMS.
- (3) NOTHING IN THE GENERAL NOTES OR SPECIAL PROVISIONS SHALL RELIEVE THE CONTRACTOR FROM HIS RESPONSIBILITIES TOWARD THE SAFETY AND CONVENIENCE OF THE GENERAL PUBLIC AND THE RESIDENTS ALONG THE PROPOSED CONSTRUCTION AREA.

## PAVEMENT MARKINGS

### TEMPORARY PAVEMENT MARKINGS ON INTERMEDIATE LAYERS

- (1) TEMPORARY PAVEMENT LINE MARKINGS ON INTERMEDIATE LAYERS OF PAVEMENT SHALL BE REFLECTIVE TAPE OR REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT, UNMARKED SECTIONS SHALL NOT BE ALLOWED. THESE MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-05.01, PAINTED PAVEMENT MARKING (4" LINE), L.M.

### FINAL PAVEMENT MARKING

- (6) THE CONTRACTOR WILL BE REQUIRED TO PERFORM THE FOLLOWING WORK:
  - a. BROOMING & DE-GRASSING SHOULDERS SHALL INCLUDE CLIPPING OF MATERIAL INTERFERING WITH PROPER DRAINAGE OF ROADWAY (INCLUDING PAVED AND GRAVEL SHOULDERS), AS DIRECTED BY THE ENGINEER.
  - b. ALL MATERIAL FROM CLIPPING, BROOMING AND DE-GRASSING SHOULDERS SHALL BE PICKED UP, REMOVED AND PROPERLY DISPOSED AS DIRECTED BY THE ENGINEER.
  - c. ALL COSTS ASSOCIATED WITH PICKING UP, REMOVAL AND PROPER DISPOSAL SHALL BE PAID FOR UNDER ITEM NO. 208-01.05.
  - d. REMOVE ALL GARBAGE AND CONSTRUCTION DEBRIS FROM PROJECT. THE COST FOR THIS WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (8) PERMANENT PAVEMENT LINE MARKINGS SHALL BE 6" ENHANCED FLATLINE THERMOPLASTIC INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-12.02, ENHANCED FLATLINE THERMO PAVT MRKNG (6IN LINE), L.M. THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK AND THEN INSTALLING THE PERMANENT MARKINGS AFTER THE PAVING OPERATION IS COMPLETED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS.

## PAVEMENT

### PAVING

- (2) THE CONTRACTOR SHALL BE REQUIRED TO COLD PLANE AND PAVE IN THE DIRECTION OF TRAFFIC.

## RESURFACING

- (4) WHERE DIRECTED BY THE TDOT ENGINEER, THE CONTRACTOR SHALL BE REQUIRED TO SHAPE PUBLIC SIDE ROADS, BUSINESS ENTRANCES, AND PRIVATE DRIVES, AS WELL AS CLEANING OF EXISTING DRAINS BEFORE PLACING MATERIALS. ALL COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (5) ALL PUBLIC SIDE ROADS SHALL BE PAVED ONE PAVER WIDTH THROUGH THE INTERSECTION AS A MINIMUM. A SATISFACTORY TRANSITION FROM THE NEW PAVEMENT TO THE EXISTING GRADE OF THE INTERSECTING PUBLIC ROAD OR BUSINESS ENTRANCE SHALL BE PROVIDED. SHOULD THE PAVEMENT OF THE INTERSECTING PUBLIC ROAD BE DISTRESSED, THE RESURFACING WIDTH MAY BE INCREASED TO THE NORMAL RIGHT OF WAY LINE.
- (6) PRIVATE DRIVEWAYS, FIELD ENTRANCES, AND BUSINESS ENTRANCES WILL BE RESURFACED A PAVER WIDTH (LANE WIDTH) AS A MINIMUM. A PAVEMENT TAPER TO TRANSITION THE NEW PAVEMENT SHALL BE REQUIRED, IT SHALL BE BASED ON AN ADDITIONAL ONE FOOT OF WIDTH PER ONE INCH DEPTH OF PAVEMENT. IF THE SHOULDER IS NARROW ENOUGH THAT THE SUM OF THE SHOULDER AND THE TRANSITION ARE LESS THAN A PAVER WIDTH, THE TRANSITION SHALL OCCUR WITHIN THE PAVER WIDTH. IF THE SUM OF THE SHOULDER AND THE TRANSITION IS GREATER THAN A PAVER WIDTH (LANE WIDTH), THE TRANSITION SHALL OCCUR OUTSIDE OF THE PAVER WIDTH.
- (7) ON CURB AND GUTTER SECTIONS, PUBLIC ROAD INTERSECTIONS SHALL BE RESURFACED TO THE END OF RADIUS. A SATISFACTORY TRANSITION FROM THE NEW PAVEMENT TO THE EXISTING GRADE OF THE INTERSECTING PUBLIC ROAD SHALL BE PROVIDED.
- (8) ON URBAN TYPICAL SECTIONS, (CURB AND GUTTER), RESIDENTIAL DRIVEWAYS AND BUSINESS ENTRANCES SHALL HAVE A MINIMUM WIDTH OF MATERIAL NOT LESS THAN ONE FOOT USED IN THE TRANSITION TO FEATHER THE PAVEMENT EDGE.
- (9) IN ALL CASES, THE LENGTH OF THE PAVEMENT TRANSITION, THE THICKNESS AND WIDTH OF THE RESURFACING AND ANY ADDITIONAL PAVEMENT MATERIALS SHALL BE AS DIRECTED BY THE TDOT ENGINEER.

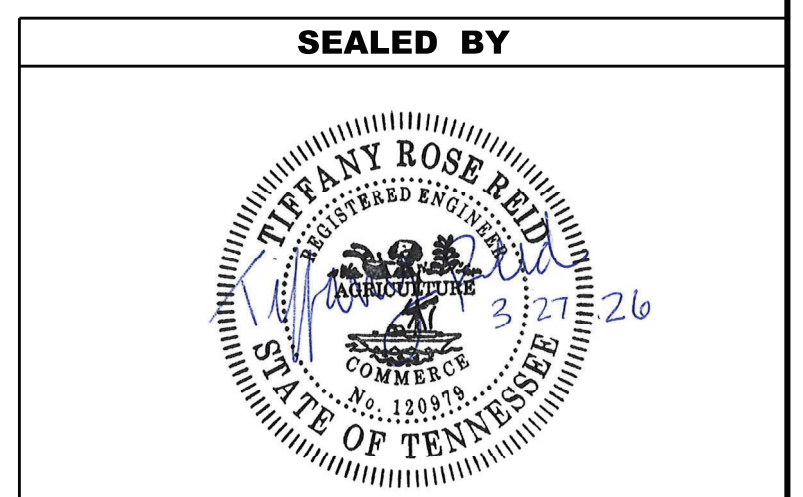
## SIGNING

- (12) ALL SIGNS WHICH INTERFERE WITH CONSTRUCTION WILL BE RELOCATED OUTSIDE LIMITS OF CONSTRUCTION BY THE CONTRACTOR. UPON COMPLETION OF CONSTRUCTION, THE CONTRACTOR WILL RESTORE THE SIGNS TO ORIGINAL LOCATION. THE CONTRACTOR SHALL CHECK WITH THE REGIONAL TRAFFIC ENGINEER PRIOR TO MOVING ANY PERMANENT SIGNS.

## CONSTRUCTION WORK ZONE & TRAFFIC CONTROL

- (1) ADVANCED WARNING SIGNS SHALL NOT BE DISPLAYED MORE THAN FORTY-EIGHT (48) HOURS BEFORE PHYSICAL CONSTRUCTION BEGINS. SIGNS MAY BE ERECTED UP TO ONE WEEK BEFORE NEEDED, IF THE SIGN FACE IS FULLY COVERED.
- (2) IF THE CONTRACTOR MOVES OFF THE PROJECT, HE SHALL COVER OR REMOVE ALL UNNEEDED SIGNS AS DIRECTED BY THE ENGINEER. COSTS OF REMOVAL, COVERING, AND REINSTALLING SIGNS SHALL NOT BE MEASURED AND PAID FOR SEPARATELY, BUT ALL COSTS SHALL BE INCLUDED IN THE ORIGINAL UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION) PER SQUARE FOOT.
- (3) A LONG TERM BUT SPORADIC USE WARNING SIGN, SUCH AS A FLAGGER SIGN, MAY REMAIN IN PLACE WHEN NOT REQUIRED PROVIDED THE SIGN FACE IS FULLY COVERED.
- (4) TRAFFIC CONTROL DEVICES SHALL NOT BE DISPLAYED OR ERECTED UNLESS RELATED CONDITIONS ARE PRESENT NECESSITATING WARNING.

- (5) USE OF BARRICADES, PORTABLE BARRIER RAILS, AND DRUMS SHALL BE LIMITED TO THE IMMEDIATE AREAS OF CONSTRUCTION WHERE A HAZARD IS PRESENT. THESE DEVICES SHALL NOT BE STORED ALONG THE ROADWAY WITHIN THIRTY (30) FEET OF THE EDGE OF THE TRAVELED WAY BEFORE OR AFTER USE UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL INCREASE TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. THESE DEVICES SHALL BE REMOVED FROM THE CONSTRUCTION WORK ZONE WHEN THE ENGINEER DETERMINES THEY ARE NO LONGER NEEDED. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.
- (6) THE CONTRACTOR SHALL NOT BE PERMITTED TO PARK ANY VEHICLES OR CONSTRUCTION EQUIPMENT DURING PERIODS OF INACTIVITY, WITHIN THIRTY (30) FEET OF THE EDGE OF PAVEMENT WHEN THE LANE IS OPEN TO TRAFFIC UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. PRIVATELY OWNED VEHICLES SHALL NOT BE ALLOWED TO PARK WITHIN THIRTY (30) FEET OF AN OPEN TRAFFIC LANE AT ANY TIME UNLESS PROTECTED AS DESCRIBED ABOVE FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.
- (7) ALL DETOUR AND CONSTRUCTION SIGNING SHALL BE IN STRICT ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- 9) THE CONTRACTOR SHALL BE RESPONSIBLE FOR STAKING CONSTRUCTION SIGNS. THE COST OF THIS WORK SHALL BE INCLUDED IN ITEM NO. 712-06, SIGNS (CONSTRUCTION), S.F.



**STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION**

GENERAL  
NOTES

# SPECIAL NOTES

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2026	STP/HSIP-2(304)	2D

## MULTIMODAL

- (1) DURING CONSTRUCTION, IF THE CONSTRUCTION ENGINEER IDENTIFIES CURB RAMP LOCATIONS WITHIN THE PROJECT LIMITS WHERE THE TDOT ROADWAY STANDARDS CANNOT BE USED DUE TO SITE LIMITATIONS, A SKETCH OR PICTURE, SHOWING EXISTING CONDITIONS AS WELL AS PROPOSED MODIFICATIONS SHOULD BE SUBMITTED TO THE REGIONAL PROJECT DEVELOPMENT OFFICE THREE WEEKS PRIOR TO THE BEGINNING OF CURB RAMP CONSTRUCTION. THE OFFICE WILL REVIEW AND EVALUATE THE LOCATIONS TO DEVELOP PROPER CURB RAMP DESIGN THAT WILL MEET REGULATIONS.

## TRAFFIC CONTROL

- (1) THE CONTRACTOR SHALL MAKE PROVISIONS TO SAFELY CONTROL TRAFFIC INGRESS AT ALL ROADWAY ENTRANCES WITHIN THE FLAGGED WORK ZONE TO THE SATISFACTION OF THE TDOT PROJECT ENGINEER. ALL COST ASSOCIATED WITH THE CONTROLS WILL BE INCLUDED IN THE PRICE BID FOR TRAFFIC CONTROL.
- (2) EXISTING CONSTRUCTION, REGULATORY AND WARNING SIGNS WHICH CONFLICT WITH THE CONSTRUCTION SIGNING SHALL BE REMOVED DURING CONSTRUCTION AND REINSTALLED AS DIRECTED BY THE TDOT PROJECT ENGINEER. ALL COSTS TO BE INCLUDED IN THE PRICE BID FOR ITEM NO. 712-01, TRAFFIC CONTROL, PER LUMP SUM.
- (3) THE CONTRACTOR SHALL GIVE THE TDOT PROJECT ENGINEER A MINIMUM OF SEVEN (7) DAYS NOTICE PRIOR TO STARTING WORK SO THAT SUFFICIENT NOTICE CAN BE PREPARED AND DISTRIBUTED TO THE MEDIA.
- (4) IMMEDIATELY UPON COMPLETION OF EACH CONSTRUCTION PHASE, ALL TRAFFIC CONTROL ITEMS THAT ARE NOT NECESSARY FOR THE SUCCEEDING PHASE SHALL BE REMOVED, COVERED OR TURNED TO FACE AWAY FROM TRAFFIC.
- (5) THE CONTRACTOR SHALL PROVIDE FLAGGER AT EACH SIDEROAD WITHIN THE PAVING OPERATION.
- (6) A MINIMUM OF ONE (1) TYPE "C" ARROW BOARD WILL BE REQUIRED. IF THE CONTRACTOR SCHEDULES HIS OPERATIONS SUCH THAT MORE THAN ONE (1) ARROW BOARD IS REQUIRED, THE COST OF ALL ADDITIONAL ARROW BOARDS WILL BE AT THE CONTRACTOR'S EXPENSE.
- (7) CHANGEABLE MESSAGE SIGNS UNITS (ITEM NO. 713-16.01) WILL BE REQUIRED ON THIS PROJECT. THE SIGNS WILL BE USED FOR ADVANCED WARNING OR AS DIRECTED BY THE ENGINEER.
- (8) MESSAGE BOARDS SHALL BE DISPLAYED A MINIMUM OF SEVEN (7) DAYS PRIOR TO STARTING WORK TO INFORM PUBLIC OF UPCOMING CONSTRUCTION PROJECT.
- (9) PRIOR TO DISPLAYING MESSAGES ON CHANGEABLE MESSAGE BOARDS, ALL MESSAGES SHALL BE APPROVED BY THE TDOT PROJECT ENGINEER AND REGION 2 TRAFFIC ENGINEER.
- (10) INFORMATION ON CHANGEABLE MESSAGE BOARDS SHALL BE UPDATED TO REFLECT CURRENT CONSTRUCTION CONDITIONS ACTIVITIES AT ALL TIMES.

## LANE CLOSURES

- (1) THE RESTRICTION OF TRAFFIC TO ONE LANE SHALL NOT EXCEED A TWO (2) MILE PAVING OPERATION OR AS DIRECTED BY THE TDOT PROJECT ENGINEER.
- (3) THE CONTRACTOR SHALL KEEP ALL TRAFFIC LANES OPEN TO TRAFFIC DURING NON-WORKING HOURS AND/OR NON-WORK DAYS.
- (4) OVERNIGHT LANE CLOSURES WILL NOT BE ALLOWED UNLESS DIRECTED BY THE TDOT PROJECT ENGINEER.
- (7) THE CONTRACTOR SHALL NOT BE ALLOWED TO INTERRUPT TRAFFIC FLOW AND SHALL MAINTAIN ALL LANES OF TRAFFIC IN EACH DIRECTION ON THE FOLLOWING DAYS:
  - A. OFFICIAL STATE HOLIDAYS.
  - C. FRIDAY AT 6:00 PM UNTIL TUESDAY AT 7:00 AM, IF A STATE HOLIDAY OCCURS OR IS OBSERVED ON MONDAY.
  - D. THURSDAY AT 6:00 PM UNTIL MONDAY AT 7:00 AM, IF A STATE HOLIDAY OCCURS OR IS OBSERVED ON FRIDAY.
  - J. DURING LOCAL FESTIVALS, GAMES OR EVENTS THAT COULD BE IMPEDED BY THE PAVING OPERATIONS WHERE AND AS DIRECTED BY THE TDOT PROJECT ENGINEER.

## PAVING

- (1) THE CONTRACTOR WILL BE REQUIRED TO PERFORM THE FOLLOWING WORK:
  - BROOMING & DE-GRASSING SHOULDERS SHALL INCLUDE CLIPPING OF MATERIAL INTERFERING WITH PROPER DRAINAGE OF ROADWAY (INCLUDING PAVED AND GRAVEL SHOULDERS), AS DIRECTED BY THE TDOT PROJECT ENGINEER.
  - ALL MATERIAL FROM CLIPPING, BROOMING AND DE-GRASSING SHOULDERS SHALL BE PICKED UP, REMOVED AND PROPERLY DISPOSED AS DIRECTED BY THE TDOT PROJECT ENGINEER.
  - ALL COSTS ASSOCIATED WITH PICKING UP, REMOVAL AND PROPER DISPOSAL SHALL BE PAID FOR UNDER ITEM NO. 208-01.05.
  - REMOVE ALL GARBAGE AND CONSTRUCTION DEBRIS FROM PROJECT. THE COST FOR THIS WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION. INTELLIGENT COMPLACTION SHALL BE USED.
- (3) ALL CATCHBASINS ARE TO BE CHECKED AND CLEANED AFTER PAVING HAS BEEN COMPLETED. NO DIRECT PAYMENT WILL BE MADE FOR THIS WORK. COST IS TO BE INCLUDED IN OTHER ITEMS OF CONSTRUCTION.
- (9) ANY QUANTITY REMAINING ON ITEMS COMPLETED PRIOR TO THE PAVING OPERATION WILL NOT BE CONVERTED TO ADDITIONAL ASPHALT FOR THE ROADWAY.

## JOINT SEALANTS

- (1) THE CONTACT SURFACE OF LONGITUDINAL JOINTS IN THE SURFACE LIFT SHALL BE SEALED BY SPRAYING THE JOINT PRIOR TO PLACEMENT OF ADDITIONAL ASPHALT AGAINST THE PREVIOUSLY PLACED MATERIAL. MANUFACTURER'S RECOMMENDATIONS SHALL BE FOLLOWED IF THE MATERIAL NEEDS TO BE RE-HEATED, AND WHEN PLACING THE THIN, UNIFORM COAT.
- (2) PRIOR TO APPLICATION OF THE SEALANT, THE FACE OF THE JOINT SHALL BE THOROUGHLY DRY AND FREE FROM DUST OR ANY OTHER MATERIAL THAT WOULD PREVENT PROPER SEALING. ALL JOINTS SHALL BE SWEEPED OR BLOWN FREE OF LOOSE MATERIAL, DIRT, VEGETATION, AND OTHER DEBRIS BY MEANS OF COMPRESSED AIR OR A POWER SWEEPER.
- (3) TRUCK AND VEHICLE TRAFFIC SHALL NOT DRIVE ACROSS A SEALED JOINT UNTIL IT HAS DRIED SUFFICIENT TO PREVENT DAMAGE FROM TRACKING.
- (4) THE TIMING OF APPLICATION OF JOINT SEALANT SHALL BE AS DIRECTED BY THE TDOT PROJECT ENGINEER.

## COLD PLANING

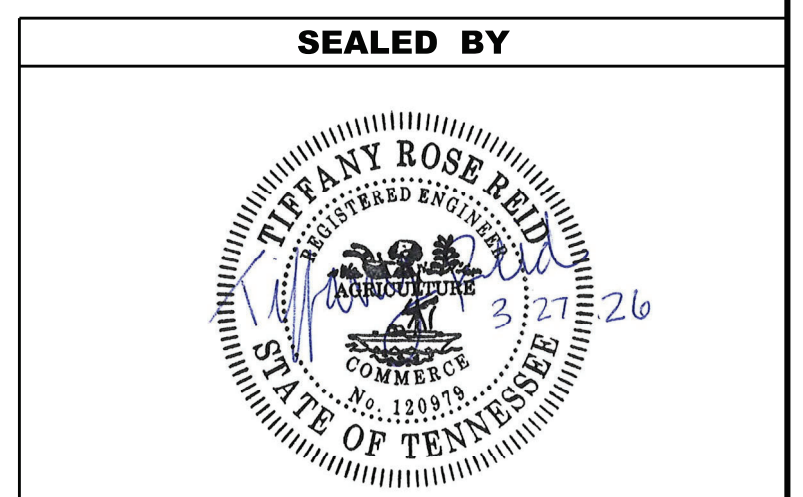
- (1) THE CONTRACTOR WILL BE REQUIRED TO PAVE THE COLD PLANED PAVEMENT WITHIN A PERIOD OF 96 HOURS.
- (2) PROVIDED THAT THE CONTRACTOR USES A COLD PLANING MACHINE WITH THE MILLING DRUM SPECIFICATIONS OF CLOSELY SPACED FINE TOOTH (5/16 INCHES OR SMALLER) MILLING DRUM, MOVING FORWARD AT MODERATE SPEED (50 TO 65 FPM.) AND THE UNDER SURFACE REMAINS INTACT (HAVING NO LOOSE MATERIAL OR GOUGES), THE CONTRACTOR MAY GET PERMISSION FROM THE REGION 2 PROJECT ENGINEER TO MILL ALL SURFACES AS NEEDED BEFORE COVERING WITH BITUMINOUS MATERIAL.
- (3) IF MILLED SURFACE BEGINS TO DETERIORATE, PAVING TO COVER UP DETERIORATING MILLED SURFACES SHOULD OCCUR AS DIRECTED BY THE TDOT PROJECT ENGINEER DURING THE NEXT WORKING DAY. IF SEVERE DISTRESS OCCURS, IMMEDIATE RESPONSE WILL BE REQUIRED.
- (4) ADDITIONAL MILLING DEPTH MAY BE REQUIRED AS DIRECTED BY THE TDOT PROJECT ENGINEER TO REDUCE SURFACE RAVELING. THE COST OF ADDITIONAL MILLING WILL BE INCLUDED IN ITEM 415-01.01, TON.
- (10) EXISTING CONCRETE POURED AROUND CATCH BASINS AT THE GUTTER SHALL NOT BE DAMAGED. THIS CONCRETE MUST REMAIN IN PLACE WITH ASPHALT TYING TO THE EXISTING CONCRETE. ANY DAMAGE WILL BE REPAIRED BY THE CONTRACTOR AT NO COST TO THE DEPARTMENT.

## PAVEMENT MARKING

- (1) THE CONTRACTOR IS RESPONSIBLE FOR THE LAYOUT OF ALL PAVEMENT MARKING. ANY CHANGE FROM THE EXISTING PAVEMENT MARKINGS MUST BE APPROVED BY THE REGION 2 TRAFFIC ENGINEER.
- (4) EXISTING PAVEMENT MARKINGS IN THE SIDE ROAD RADII SHALL BE RE-TRACED AS DIRECTED BY THE TDOT PROJECT ENGINEER DURING FINAL STRIPING.

## MISCELLANEOUS

- (1) ALL SAFETY IMPROVEMENTS TO BE PAID FOR UNDER PROJECT NUMBER: 58S002-F3-007
- (2) ALL LANE CLOSURES AND OPERATIONS MUST BE COORDINATED WITH EXISTING CONSTRUCTION CONTRACTS IN THE AREA.



STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

SPECIAL  
NOTES

# ENVIRONMENTAL NOTES

## ENVIRONMENTAL GENERAL NOTES

### NATURAL RESOURCES

- (4) THE OPERATION OF EQUIPMENT IN WATERS OF THE STATE/U.S., INCLUDING WETLANDS AND EPHEMERAL, INTERMITTENT, AND PERENNIAL STREAMS, IS NOT ALLOWED.
- (9) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS PRIOR TO ANY CONSTRUCTION AND MAINTENANCE ACTIVITIES TO ENSURE THAT ENVIRONMENTAL FEATURES (E.G., STREAMS, WETLANDS, SPRINGS, ETC.) ARE NOT IMPACTED BEYOND PERMITTED LOCATIONS. IF THE CONTRACTOR OR TDOT INSPECTOR IS UNSURE OF THE IDENTITY OF AN ENVIRONMENTAL FEATURE, THE INSPECTOR SHALL CONTACT THE TDOT REGION ENVIRONMENTAL TECH GROUP IMMEDIATELY.

### SPECIES

- (11) SHOULD CLIFF SWALLOW OR BARN SWALLOW NESTS, EGGS, OR BIRDS (YOUNG AND ADULTS) BE PRESENT, THE CONTRACTOR SHALL CONTACT THE REGIONAL ECOLOGY OFFICE TO DETERMINE IF SEASONAL RESTRICTIONS WILL BE NECESSARY. GENERALLY, BIRDS, NESTS, AND EGGS MAY NOT BE DISTURBED BETWEEN APRIL 15 AND JULY 31. FROM AUGUST 1 TO APRIL 14, NESTS CAN BE REMOVED OR DESTROYED SO LONG AS BIRDS OR EGGS ARE NOT PRESENT, AND MEASURES IMPLEMENTED TO PREVENT FUTURE NEST BUILDING AT THE SITE (I.E., CLOSING OFF AREA USING NETTING).
- (12) IF THE REMOVAL OF ANY TREES WITH A DIAMETER AT BREAST HEIGHT (DBH) GREATER THAN 3 INCHES IS DEEMED NECESSARY THE TDOT SUPERVISOR SHALL CONTACT THE TDOT ENVIRONMENTAL DIVISION, ECOLOGY SECTION IMMEDIATELY.

### PERMITS, PLANS & RECORDS

- (15) IF A CHANGE IN PROJECT SCOPE OCCURS DURING CONSTRUCTION, INCLUDING VALUE ENGINEERING, THE TDOT PERMIT SECTION SHALL BE CONTACTED TO DETERMINE WHETHER PERMIT REVISIONS ARE NEEDED. THE PROJECT MANAGER SHALL BE CONTACTED TO DETERMINE IF ANY PLAN REVISIONS ARE NEEDED.

## ENVIRONMENTAL SPECIAL NOTES

### ENVIRONMENTAL

- (1) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION COMPLIANCE AND FIELD SERVICES OFFICE SHALL BE INVITED TO FIELD REVIEW MEETINGS.

### PROJECT COMMITMENTS

- (5) SEE PROJECT COMMITMENTS, SHEET 1B, FOR DETAILS RELATING TO SPECIAL ENVIRONMENTAL COMMITMENTS REQUIRED BY THIS PROJECT.

### SCOPE OF WORK

- (6) MILL, CS, 411D, INTELLIGENT COMPACTION, PAVEMENT MARKINGS, AND CURB RAMPS.

## EROSION PREVENTION AND SEDIMENT CONTROL GENERAL NOTES

### DISTURBED AREA

- (1) IF DISTURBED ACREAGE IS EQUAL TO ONE ACRE OR MORE, PLEASE CONTACT TDOT ENVIRONMENTAL DIVISION, PERMITS SECTION AS SOON AS POSSIBLE BECAUSE AN NPDES PERMIT WILL BE REQUIRED.

### SEDIMENT CONTROL

- (6) EPSC MEASURES SHALL BE INSTALLED AND FUNCTIONAL PRIOR TO ANY EARTH MOVING OPERATIONS AND SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD EXCEPT AS SUCH WORK MAY BE NECESSARY TO INSTALL EPSC MEASURES.

- (8) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT THE OFFSITE MIGRATION OR DEPOSIT OF SEDIMENT OFF THE PROJECT LIMITS (E.G. R.O.W., EASEMENTS, ETC.), INTO WATERS OF THE STATE/U.S., OR ONTO ROADWAYS USED BY THE GENERAL PUBLIC. IF SEDIMENT ESCAPES THE CONSTRUCTION SITE, OFFSITE ACCUMULATIONS OF SEDIMENT THAT HAVE NOT REACHED A STREAM MUST BE REMOVED AT A FREQUENCY SUFFICIENT TO MINIMIZE OFFSITE IMPACTS (E.G., FUGITIVE SEDIMENT THAT HAS ESCAPED THE CONSTRUCTION SITE AND HAS COLLECTED IN A STREET MUST BE REMOVED SO THAT IT IS NOT SUBSEQUENTLY WASHED INTO STORM SEWERS AND STREAMS BY THE NEXT RAIN AND/OR SO THAT IT DOES NOT POSE A SAFETY HAZARD TO USERS OF PUBLIC STREETS). ARRANGEMENTS CONCERNING REMOVAL OF SEDIMENT ON ADJOINING PROPERTY MUST BE NEGOTIATED WITH THE ADJOINING PROPERTY OWNER BEFORE REMOVAL OF SEDIMENT.

### GOOD HOUSEKEEPING MEASURES & WASTE DISPOSAL

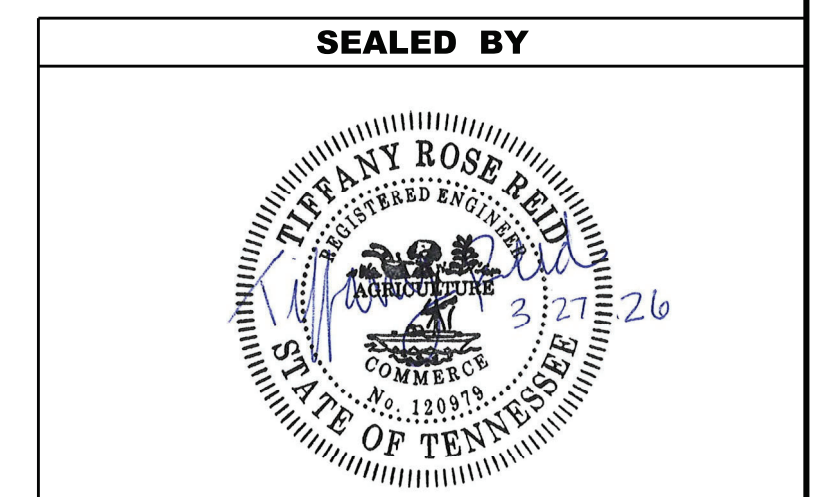
- (29) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT LITTER AND CONSTRUCTION WASTES FROM ENTERING WATERS OF THE STATE/U.S. THESE MATERIALS SHALL BE REMOVED FROM STORMWATER EXPOSURE PRIOR TO ANTICIPATED STORM EVENTS OR BEFORE BEING CARRIED OFFSITE BY WIND, OR OTHERWISE PREVENTED FROM BECOMING A POLLUTANT SOURCE FOR STORMWATER DISCHARGES. AFTER USE, MATERIALS USED FOR EPSC SHALL BE REMOVED FROM THE SITE.
- (30) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS TO ENSURE THAT PETROLEUM PRODUCTS OR OTHER CHEMICAL POLLUTANTS ARE PREVENTED FROM ENTERING WATERS OF THE STATE/U.S. ALL EQUIPMENT REFUELING, SERVICING, AND STAGING AREAS SHALL COMPLY WITH ALL LOCAL, STATE, AND FEDERAL LAWS, RULES, REGULATIONS, AND ORDINANCES, INCLUDING THOSE OF THE NATIONAL FIRE PROTECTION ASSOCIATION. APPROPRIATE CONTAINMENT MEASURES FOR THESE AREAS SHALL BE USED.
- (31) CONTRACTORS SHALL PROVIDE DESIGNATED TRUCK WASHOUT AREAS ON THE SITE. THESE AREAS MUST BE SELF CONTAINED, NOT CONNECTED TO ANY STORMWATER OUTLET OF THE SITE, AND PROPERLY SIGNED. WASH DOWN OR WASTE DISCHARGE OF CONCRETE TRUCKS SHALL NOT BE PERMITTED ONSITE UNLESS PROPER SETTLEMENT AREAS HAVE BEEN PROVIDED IN ACCORDANCE WITH BOTH STATE AND FEDERAL REGULATIONS.
- (32) WHEEL WASH WATER SHALL BE COLLECTED AND ALLOWED TO SETTLE OUT SUSPENDED SOLIDS PRIOR TO DISCHARGE. WHEEL WASH WATER SHALL NOT BE DISCHARGED DIRECTLY INTO ANY STORMWATER SYSTEM OR STORMWATER TREATMENT SYSTEM.
- (33) IF PORTABLE SANITARY FACILITIES ARE PROVIDED ON CONSTRUCTION SITES, SANITARY WASTE SHALL BE COLLECTED FROM THE PORTABLE UNITS IN A TIMELY MANNER BY A LICENSED WASTE MANAGEMENT CONTRACTOR OR AS REQUIRED BY ANY REGULATIONS. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF SANITARY WASTE.
- (34) ONLY CONSTRUCTION PRODUCTS NEEDED SHALL BE STORED ONSITE BY THE CONTRACTOR. THE CONTRACTOR SHALL STORE ALL MATERIALS UNDER COVER AND IN APPROPRIATE CONTAINERS. PRODUCTS MUST BE STORED IN ORIGINAL CONTAINERS AND LABELED. MATERIAL MIXING SHALL BE CONDUCTED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS. THE CONTRACTOR'S RESPONSIBLE PARTY SHALL INSPECT MATERIALS STORAGE AREAS REGULARLY TO ENSURE PROPER USE AND DISPOSAL.
- (35) WHEN POSSIBLE, ALL PRODUCTS SHALL BE USED COMPLETELY BEFORE PROPERLY DISPOSING OF THE CONTAINER OFFSITE. THE MANUFACTURER'S DIRECTIONS FOR DISPOSAL OF MATERIALS AND CONTAINERS SHALL BE FOLLOWED.
- (36) ALL PAINT CONTAINERS SHALL BE TIGHTLY SEALED AND STORED WHEN NOT REQUIRED FOR USE. EXCESS PAINT SHALL BE DISPOSED OF ACCORDING TO THE MANUFACTURER'S INSTRUCTIONS AND APPLICABLE STATE AND LOCAL REGULATIONS.
- (37) ALL HAZARDOUS WASTE MATERIALS SHALL BE DISPOSED OF IN A MANNER WHICH IS COMPLIANT WITH LOCAL OR STATE REGULATIONS. SITE PERSONNEL SHALL BE INSTRUCTED IN THESE PRACTICES, AND THE INDIVIDUAL DESIGNATED AS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR SEEING THAT THESE PRACTICES ARE FOLLOWED. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF HAZARDOUS MATERIAL.

- (38) OPEN BURNING IS PROHIBITED UNLESS IT IS SPECIFICALLY ALLOWED BY LAW. IF ALLOWED, NATURAL VEGETATION, TREES, AND UNTREATED LUMBER SHALL BE THE ONLY MATERIALS THAT CAN BE OPEN BURNED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL APPLICABLE STATE AND LOCAL PERMITS PRIOR TO ANY BURNING.
- (39) DISPOSAL OF ONSITE VEGETATION AND TREES BY CHIPPING THEM INTO MULCH IS PREFERABLE TO OPEN BURNING. THIS MULCH MAY BE USED AS AN ONSITE SOIL STABILIZATION MEASURE WHERE APPROPRIATE.
- (40) WASTE MATERIAL (EARTH, ROCK, ASPHALT, CONCRETE, ETC.) NOT REQUIRED FOR THE CONSTRUCTION OF THE PROJECT WILL BE DISPOSED OF BY THE CONTRACTOR. IMPACTS TO WATERS OF THE STATE/U.S. SHALL BE AVOIDED IF POSSIBLE. IF UNAVOIDABLE, THE CONTRACTOR WILL OBTAIN ANY AND ALL NECESSARY PERMITS INCLUDING, BUT NOT LIMITED TO NPDES, AQUATIC RESOURCES ALTERATION PERMIT(S), CORPS OF ENGINEERS SECTION 404 PERMITS, AND TVA SECTION 26A PERMITS TO DISPOSE OF WASTE MATERIALS.

### SPILL PREVENTION, MANAGEMENT & NOTIFICATION

- (44) ALL ONSITE VEHICLES SHALL BE MONITORED FOR LEAKS AND RECEIVE REGULAR PREVENTIVE MAINTENANCE TO REDUCE THE CHANCE OF LEAKAGE AND SPILLS.
- (45) FOR ALL HAZARDOUS MATERIALS STORED ONSITE, THE MANUFACTURER'S RECOMMENDED METHODS FOR SPILL CLEAN UP SHALL BE CLEARLY POSTED. SITE PERSONNEL SHALL BE MADE AWARE OF THE PROCEDURES AND THE LOCATIONS OF THE INFORMATION AND CLEANUP SUPPLIES.
- (46) APPROPRIATE CLEANUP MATERIALS AND EQUIPMENT SHALL BE MAINTAINED BY THE CONTRACTOR IN THE MATERIALS STORAGE AREA ONSITE AND UNDER COVER. SPILL RESPONSE EQUIPMENT SHALL BE INSPECTED AND MAINTAINED BY THE CONTRACTOR AS NECESSARY TO REPLACE ANY MATERIALS USED IN SPILL RESPONSE ACTIVITIES.
- (47) ALL SPILLS SHALL BE CLEANED IMMEDIATELY AFTER DISCOVERY AND THE MATERIALS DISPOSED OF PROPERLY. THE SPILL AREA SHALL BE KEPT WELL VENTILATED AND PERSONNEL WILL WEAR APPROPRIATE PROTECTIVE CLOTHING TO PREVENT INJURY FROM CONTACT WITH A HAZARDOUS SUBSTANCE.
- (48) THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE THE SPILL PREVENTION AND CLEANUP COORDINATOR. THE CONTRACTOR IS RESPONSIBLE FOR ENSURING THAT THE SITE SUPERINTENDENT HAS HAD APPROPRIATE TRAINING FOR HAZARDOUS MATERIALS HANDLING, SPILL MANAGEMENT, AND CLEANUP.
- (49) IF AN OIL SHEEN IS OBSERVED ON SURFACE WATER (E.G. SETTLING PONDS, DETENTION PONDS, SWALES), ACTION SHALL BE TAKEN IMMEDIATELY TO REMOVE THE MATERIAL CAUSING THE SHEEN. THE CONTRACTOR SHALL USE APPROPRIATE MATERIALS TO CONTAIN AND ABSORB THE SPILL. THE SOURCE OF THE OIL SHEEN WILL ALSO BE IDENTIFIED AND REMOVED OR REPAIRED AS NECESSARY TO PREVENT FURTHER RELEASES.
- (50) FERTILIZERS SHALL BE APPLIED ONLY IN THE AMOUNTS SPECIFIED. ONCE APPLIED, FERTILIZERS SHALL BE WORKED INTO THE SOIL TO LIMIT THE EXPOSURE TO STORMWATER.
- (51) IF A SPILL OCCURS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR COMPLETING THE SPILL REPORTING FORM AND FOR REPORTING THE SPILL TO THE TDOT PROJECT RESPONSIBLE PARTY. ALL SPILLS MUST BE REPORTED TO THE APPROPRIATE AGENCY, AND MEASURES SHALL BE TAKEN IMMEDIATELY TO PREVENT THE POLLUTION OF WATERS OF THE STATE/U.S., INCLUDING GROUNDWATER, SHOULD A SPILL OCCUR.
- (52) WHERE A RELEASE CONTAINING A HAZARDOUS SUBSTANCE IN AN AMOUNT EQUAL TO OR IN EXCESS OF A REPORTABLE QUANTITY ESTABLISHED UNDER EITHER 40 CFR 117 OR 40 CFR 302 OCCURS DURING A 24 HOUR PERIOD, SEE THE LATEST TENNESSEE GENERAL PERMIT NO. TNR100000 STORMWATER DISCHARGES FROM CONSTRUCTION ACTIVITIES SECTION 5.1 FOR REPORTING REQUIREMENTS.
- (53) CONTRACTOR'S BULK FUEL AND PETROLEUM PRODUCTS STORED ONSITE OR ADJACENT TO THE R.O.W. IN ABOVE GROUND STORAGE CONTAINERS WITH A COMBINED CAPACITY OF 1320 GALLONS OR MORE SHALL HAVE SECONDARY CONTAINMENT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PREPARING A SPILL PREVENTION CONTROL AND COUNTERMEASURE (SPCC) PLAN FOR THE BULK STORAGE AND BE SOLELY RESPONSIBLE FOR OBTAINING ANY NECESSARY LOCAL, STATE, AND FEDERAL PERMITS. THE SPCC PLAN AND/OR PERMITS SHALL BE KEPT ONSITE AND A COPY PROVIDED TO THE TDOT PROJECT RESPONSIBLE PARTY PRIOR TO STORING 1320 GALLONS ON SITE.

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2026	STP/HSP-2(304)	2E



STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

ENVIRONMENTAL  
NOTES

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2026	STP/HSP-2(304)	2F

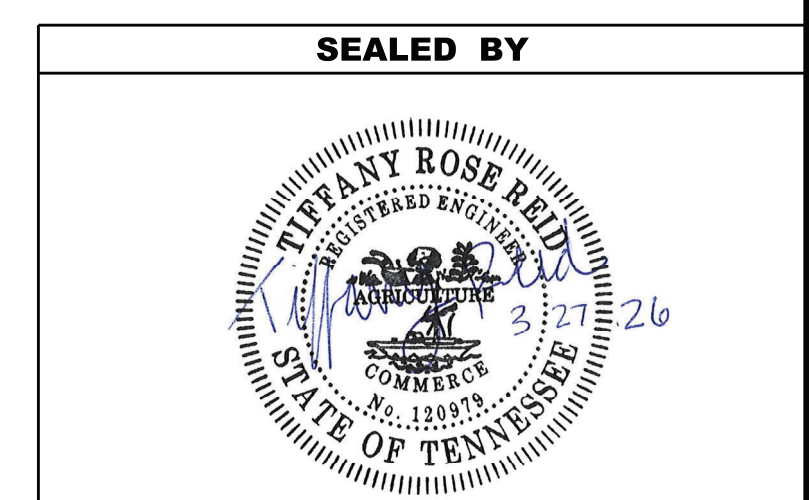
TRAFFIC CONTROL SIGN TABULATION							
M.U.T.C.D. SIGN NO.	LEGEND	SIZE IN INCHES			S.F.	TOTAL	ITEM NO.
		L	X	W		NO. REQUIRED	712-06 S.F.
G20-1	ROAD WORK NEXT 2 MILES	36"	x	18"	5	2	9
G20-2	END ROAD WORK	36"	x	18"	5	20	90
W3-4	BE PREPARED TO STOP	36"	x	36"	9	2	18
W4-2R	LANE ENDS	36"	x	36"	9	1	9
W8-11	UNEVEN LANES	36"	x	36"	9	4	36
W8-15	GROOVED PAVEMENT	36"	x	36"	9	4	36
W8-15aP	MOTORCYCLE (PLAQUE)	24"	x	18"	3	4	12
W20-1	ROAD WORK AHEAD	36"	x	36"	9	18	162
W20-1	ROAD WORK 1 MILE	36"	x	36"	9	2	18
W20-1	ROAD WORK 1/2 MILE	36"	x	36"	9	2	18
W20-1	ROAD WORK 1000'	36"	x	36"	9	2	18
W20-5R	RIGHT LANE CLOSED AHEAD	36"	x	36"	9	1	9
W20-7	FLAGGER	36"	x	36"	9	2	18
W21-2	FRESH OIL	36"	x	36"	9	4	36
<b>TOTAL</b>					<b>489</b>	<b>S.F.</b>	

THIS CONSTRUCTION SIGNING IS TO BE CONSTRUCTED AS A MINIMUM.  
OTHER SIGNS AS DIRECTED BY THE ENGINEER MAY BE REQUIRED DURING  
DIFFERENT PHASES.

UTILITY ADJUSTMENTS (BY OTHERS)			
TYPE	NUMBER	OWNERS/ CONTACT	PHONE #
MANHOLES	8	CITY OF MONTEAGLE- EARL GEARY	(423) 605-3825

GUARDRAIL ADJUSTMENTS (RESURFACING)
THERE ARE NO GUARDRAIL ADJUSTMENTS WITHIN PROJECT LIMITS

BRIDGE DECK RECOMMENDATIONS (RESURFACING)				
BRIDGE NUMBER	LOCATION LOG MILE	CROSS OVER / UNDER	BRIDGE LENGTH	BRIDGE DECK RECOMMENDATIONS
58I00240071	1.40	I-24 WB LNS	166' 3 1/2"	REMOVE ASPHALT (IF PRESENT)- PROVIDE DECK REPAIR ITEMS- SEAL WITH MEMBRANE (BRIDGE SHEETS INCLUDED)
58I00240007	0.011	I-24 EB LNS	208' 2"	REMOVE ASPHALT (IF PRESENT)- PROVIDE DECK REPAIR ITEMS- SEAL WITH MEMBRANE (BRIDGE SHEETS INCLUDED)



**STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION**

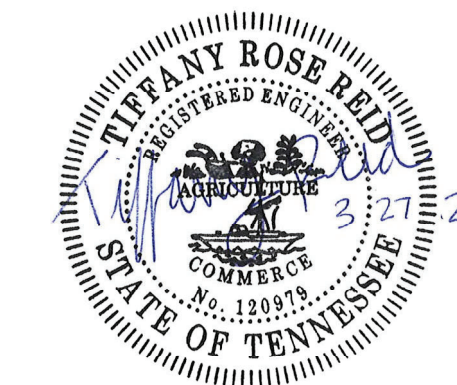
TABULATED  
QUANTITIES

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2026	STP/HSP-2(304)	2F1

### CURB RAMP TABULATION

ROADWAY		LOCATION				QUADRANT				SIGNALIZED YES / NO	STANDARD DRAWING NO.	CONCRETE SIDEWALK 4" 701-01.01	CURB RAMP (RETROFIT) ITEM NO. 701-02.01 S.F.	CURB RAMP (NEW) ITEM NO. 701-02.03 S.F.	REMARKS	
MAINLINE	INTERSECTING	STATION or LOG MILE (L.M.)	LEFT	MEDIAN	RIGHT	N.	S.	E.	W.							
SR2	SAMPLEY ST.	0.99			X		X	X		N	RP-H-7		75		NEEDS DOME AND TURN SPACE OUT OF SPEC	
SR2	DUBOSE ST.	0.82			X	X		X		N	RP-H-6		25		DOME LANDING 5% OUT OF SPEC	
SR2	DUBOSE ST.	0.82			X	X			X	N	RP-H-7		150		OUT OF SPEC	
SR2	FAIRMOUNT ST.	0.79			X	X		X		N	RP-H-6		25		NO DOME	
SR2	SCHOOL CROSSING	0.56	X				X		X	N	RP-H-5	50	100		NO DOME	
SR2	SCHOOL CROSSING	0.56			X	X	X			N	RP-H-8	50		45	RAMP NEEDS MOVED WEST 17-20 FT. CHECK ROW	
SR2	S. CENTRAL AVE	0.39	X				X	X		N	CR-40		50		NO DOME	
SR2	S. CENTRAL AVE	0.39	X				X		X	N	CR-40		75		NO DOME	
SR2	SPRING ST.	0.46	X				X	X		N	CR-40		50		NO DOME	
SR2	SPRING ST.	0.46	X				X		X	N	CR-40		50		NO DOME	
												<b>TOTAL</b>	<b>100</b>	<b>600</b>	<b>45</b>	

SEALED BY



STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

TABULATED  
QUANTITIES

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2026	STP/HSIP-2(304)	2G



N. CENTRAL AVE.

SR-2

S. CENTRAL AVE.

SEALED BY

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

DETAIL SHEET

NOT TO SCALE  
SHEET 1 OF 5

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2026	STP/HSIP-2(304)	2G1



SEALED BY

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

DETAIL SHEET

NOT TO SCALE  
SHEET 2 OF 5



TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2026	STP/HSIP-2(304)	2G2

<b>SEALED BY</b>

<b>STATE OF TENNESSEE</b> <b>DEPARTMENT OF TRANSPORTATION</b>
<b>DETAIL SHEET</b>
<small>NOT TO SCALE</small> <small>SHEET 3 OF 5</small>

3/11/2026 9:50:00 AM G:\25-0267\PLAN SHEETS\2026.05 LETTINGS\134114.00 SR 2\TEST.DGN



TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2026	STP/HSIP-2(304)	2G3

<b>SEALED BY</b>

<b>STATE OF TENNESSEE</b> <b>DEPARTMENT OF TRANSPORTATION</b>
<b>DETAIL SHEET</b>
<small>NOT TO SCALE</small> <small>SHEET 4 OF 5</small>

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2026	STP/HSIP-2(304)	2G4



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SEALED BY

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

DETAIL SHEET

NOT TO SCALE  
SHEET 5 OF 5

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2026	STP/HSIP-2(304)	3

**UTILITY NOTES**

- (2) UNLESS OTHERWISE NOTED, ALL UTILITY ADJUSTMENTS WILL BE PERFORMED BY THE UTILITY OR ITS REPRESENTATIVE. THE CONTRACTOR AND UTILITY OWNERS WILL BE REQUIRED TO COOPERATE WITH EACH OTHER IN ORDER TO EXPEDITE THE WORK REQUIRED BY THIS CONTRACT. ON CONTRACTS WHERE CONSTRUCTION STAKES, LINES, AND GRADES ARE CONTRACT ITEMS, THE CONTRACTOR WILL BE REQUIRED TO PROVIDE RIGHT-OF-WAY OR SLOPE STAKES, DITCH OR STREAM BED GRADES, OR OTHER ESSENTIAL SURVEY STAKING TO PREVENT CONFLICTS WITH THE HIGHWAY CONSTRUCTION. FREQUENTLY, THIS WILL BE REQUIRED AS THE FIRST ITEM OF WORK AND AT ANY LOCATION ON THE PROJECT DIRECTED BY THE ENGINEER.
- (3) THE CONTRACTOR WILL PROVIDE ALL NECESSARY PROTECTIVE MEASURES TO SAFEGUARD EXISTING UTILITIES FROM DAMAGE DURING CONSTRUCTION OF THIS PROJECT. IN THE EVENT THAT SPECIAL EQUIPMENT IS REQUIRED TO WORK OVER AND AROUND THE UTILITIES, THE CONTRACTOR WILL BE REQUIRED TO FURNISH SUCH EQUIPMENT. THE COST OF PROTECTING UTILITIES FROM DAMAGE AND FURNISHING SPECIAL EQUIPMENT WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (4) PRIOR TO SUBMITTING HIS BID, THE CONTRACTOR WILL BE SOLELY RESPONSIBLE FOR CONTACTING OWNERS OF ALL AFFECTED UTILITIES IN ORDER TO DETERMINE THE EXTENT TO WHICH UTILITY RELOCATIONS AND/OR ADJUSTMENTS WILL HAVE UPON THE SCHEDULE OF WORK FOR THE PROJECT. WHILE SOME WORK MAY BE REQUIRED 'AROUND' UTILITY FACILITIES THAT WILL REMAIN IN PLACE, OTHER UTILITY FACILITIES MAY NEED TO BE ADJUSTED CONCURRENTLY WITH THE CONTRACTOR'S OPERATIONS. ADVANCE CLEAR CUTTING MAY BE REQUIRED BY THE ENGINEER AT ANY LOCATION WHERE CLEARING IS CALLED FOR IN THE SPECIFICATIONS AND CLEAR CUTTING IS NECESSARY FOR A UTILITY RELOCATION. ANY ADDITIONAL COST WILL BE INCLUDED IN THE UNIT PRICE BID FOR THE CLEARING ITEM SPECIFIED IN THE PLANS.
- (5) THE CONTRACTOR SHALL NOTIFY EACH INDIVIDUAL UTILITY OWNER OF HIS PLAN OF OPERATION IN THE AREA OF THE UTILITIES. PRIOR TO COMMENCING WORK, THE CONTRACTOR SHALL CONTACT THE UTILITY OWNERS AND REQUEST THEM TO PROPERLY LOCATE THEIR RESPECTIVE UTILITY ON THE GROUND. THIS NOTIFICATION SHALL BE GIVEN AT LEAST THREE (3) BUSINESS DAYS PRIOR TO COMMENCEMENT OF OPERATIONS AROUND THE UTILITY IN ACCORDANCE WITH TCA 65-31-106. NOTIFICATION BY CALLING THE TENNESSEE ONE CALL SYSTEM, INC AT 1-800-351-1111 WILL BE REQUIRED.

**UTILITY OWNERS**

**ELECTRIC:**  
**SEQUACHEE VALLEY ELECTRIC COOPERATIVE**  
 512 S. CEDAR AVENUE  
  
 SOUTH PITTSBURG, TN 37380  
 CONTACT: LUCAS MARSH  
 OFFICE PHONE: (423) 837-8605  
 CELL PHONE: N/A  
 Email: lmarsh@svalleyec.com

**TELEPHONE:**  
**BELLSOUTH dba AT&T**  
 300 E MARTIN LUTHER KING BOULEVARD, 5<sup>TH</sup> FLOOR  
 CHATTANOOGA, TN 37403  
 CONTACT: JOE PERREL  
 OFFICE PHONE: (423) 266-1566  
 CELL PHONE: N/A  
 Email: Jp1389@att.com

**FIBER:**  
**SEQUACHEE VALLEY ELECTRIC COOPERATIVE**  
 PO BOX 31  
 SOUTH PITTSBURG, TN 37380  
 CONTACT: MIKE BIRDWELL  
 OFFICE PHONE: (423) 837-5076  
 CELL PHONE: N/A  
 Email: mbirdwell@svalleyec.com

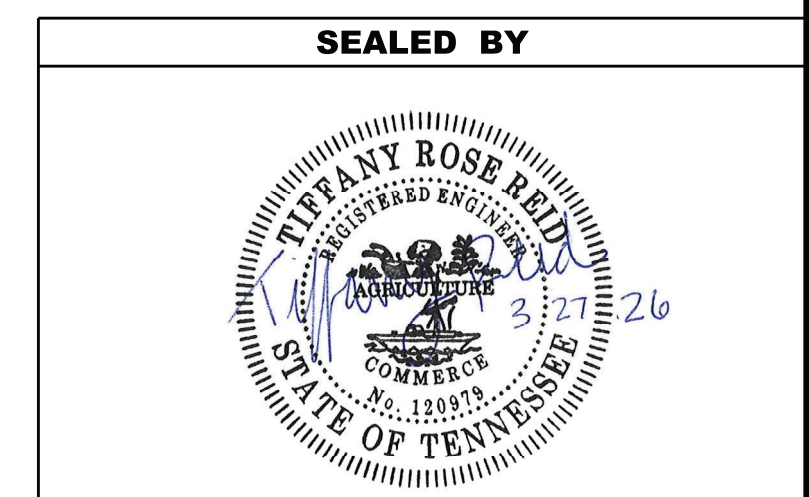
**TELEPHONE:**  
**CENTURYLINK/LUMEN**  
 2530 PERIMETER PLACE DRIVE  
 NASHVILLE, TN 37214  
 CONTACT: PLANS DISTRIBUTION  
 OFFICE PHONE: (615) 263-1128  
 CELL PHONE: N/A  
 Email: relocations@lumen.com

**GAS:**  
**MIDDLE TENNESSEE NATURAL GAS**  
 PO BOX 670  
 SMITHVILLE, TN 37166  
 CONTACT: MATT STENNETT  
 OFFICE PHONE: (931) 754-3515  
 CELL PHONE: N/A  
 Email: mstennet@mtng.com

**WATER:**  
**TOWN OF MONTEAGLE**  
 PO BOX 127  
 MONTEAGLE, TN 37356  
 CONTACT: GREG MALOOF  
 OFFICE PHONE: (931) 924-2265  
 CELL PHONE: N/A  
 Email: monteagleinfo@benlomand.net

**GAS:**  
**TOWN OF MONTEAGLE**  
 PO BOX 127  
 MONTEAGLE, TN 37356  
 CONTACT: GREG MALOOF  
 OFFICE PHONE: (931) 924-2265  
 CELL PHONE: N/A  
 Email: monteagleinfo@benlomand.net

**SEWER:**  
**TOWN OF MONTEAGLE**  
 PO BOX 127  
 MONTEAGLE, TN 37356  
 CONTACT: GREG MALOOF  
 OFFICE PHONE: (931) 924-2256  
 CELL PHONE: N/A  
 Email: Monteagleinfo@benlomand.net



**STATE OF TENNESSEE  
 DEPARTMENT OF TRANSPORTATION**

**UTILITY NOTES  
 AND  
 UTILITY OWNERS**

# PAVEMENT EDGE DROP-OFF TRAFFIC CONTROL NOTES

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2026	STP/HSP-2(304)	T1

## A. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES OR TRAFFIC LANE AND SHOULDER WHERE THE TRAFFIC LANE IS BEING USED BY TRAFFIC, CAUSED BY BASE, PAVING OR RESURFACING:

1. DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 0.75 INCH AND NOT EXCEEDING 1.75 INCHES:
  - a. WARNING SIGNS, UNEVEN LANES (W8-11) AND/OR SHOULDER DROP-OFF WITH PLAQUE (W8-17 AND W8-17P), SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.
  - b. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY ADDED PAVEMENT SHALL BE ELIMINATED WITHIN THREE WORKDAYS.
  - c. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY COLD PLANING SHALL BE ELIMINATED WITHIN THREE WORKDAYS.
  - d. WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE TRAFFIC LANE BEING UTILIZED BY TRAFFIC AND SHOULDER THE DIFFERENCE IN ELEVATION SHALL BE ELIMINATED WITHIN SEVEN WORKDAYS AFTER THE CONDITION IS CREATED.
2. DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 1.75 INCHES AND NOT EXCEEDING 6 INCHES, TRAFFIC IS NOT TO BE ALLOWED TO TRAVERSE THIS DIFFERENCE IN ELEVATION.
  - a. SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:
    - (1) WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.
    - (2) WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.
  - b. IF THE DIFFERENCE IN ELEVATION IS ELIMINATED OR DECREASED TO 2 INCHES OR LESS BY THE END OF EACH WORKDAY, CONES MAY BE USED DURING DAYLIGHT HOURS IN LIEU OF DRUMS, BARRICADES OR OTHER APPROVED PROTECTIVE DEVICES MENTIONED IN PARAGRAPH a, PROVIDED WARNING SIGNS ARE ERECTED. WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.
  - c. WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE THROUGH TRAFFIC LANE AND THE SHOULDER AND THE ELEVATION DIFFERENCE IS LESS THAN 3 INCHES, THE CONTRACTOR MAY USE WARNING SIGNS AND/OR PROTECTIVE DEVICES AS APPLICABLE AND APPROVED BY THE REGIONAL TRAFFIC ENGINEER. SEE PARAGRAPH a REGARDING USE OF DRUMS, BARRICADES OR OTHER APPROVED PROTECTIVE DEVICES. WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) WILL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

IN THESE SITUATIONS, THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 2 MILES IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.

## 3. DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 6 INCHES BUT NOT EXCEEDING 18 INCHES, THE CONTRACTOR, WITH THE ENGINEER'S APPROVAL, MAY UTILIZE ONE OF THE FOLLOWING:

- a. THE CONTRACTOR SHALL ACCOMPLISH SEPARATION BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:
  - (1) WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.
  - (2) WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

IN ORDER TO USE THIS METHOD, THE CONTRACTOR MUST REDUCE THE DIFFERENCE IN ELEVATION TO 6 INCHES OR LESS BY THE END OF THE WORKDAY THAT THE CONDITION IS CREATED.

- b. THE CONTRACTOR SHALL PROVIDE DRUMS, BARRICADES OR OTHER APPROVED SEPARATION DEVICES AS SPECIFIED IN PARAGRAPH a, AND CONSTRUCT A STONE WEDGE WITH A 4:1 SLOPE, OR FLATTER, TO ELIMINATE THE VERTICAL OFFSET IF THE LOWER ELEVATION IS AT OR BELOW SUBGRADE AT THE END OF EACH DAY.
- c. THE CONTRACTOR SHALL PROVIDE DRUMS, BARRICADES OR OTHER APPROVED SEPARATION DEVICES AS SPECIFIED IN PARAGRAPH a AND IF THE LOWER ELEVATION IS BASE STONE OR ASPHALT PAVEMENT, PLACEMENT OF SUBSEQUENT LAYERS OF PAVEMENT MUST BEGIN THE NEXT WORK DAY AND PROGRESS CONTINUOUSLY UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED OR REDUCED TO 6 INCHES OR LESS.
- d. THE CONTRACTOR SHALL PROVIDE SEPARATION BY PORTABLE BARRIER RAIL.

FOR PRECEDING CONDITIONS a, b, AND c, THE CONTRACTOR SHALL USE THE SHOULDER DROP-OFF WARNING SIGN WITH PLAQUE (W8-17 AND W8-17P). IT SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN THE SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. IN THESE SITUATIONS, THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 1 MILE IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.

## 4. FOR DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 18 INCHES.

SEPARATION WILL BE PROVIDED BY USE OF PORTABLE BARRIER RAIL.

IN THIS SITUATION THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 1 MILE IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.

## B. IF THE DIFFERENCE IN ELEVATION IS WITHIN 30 FEET OF THE NEAREST TRAFFIC LANE BEING USED BY TRAFFIC CAUSED BY GRADING, EXCAVATION FOR UTILITIES, DRAINAGE STRUCTURES, UNDERCUTTING, ETC.:

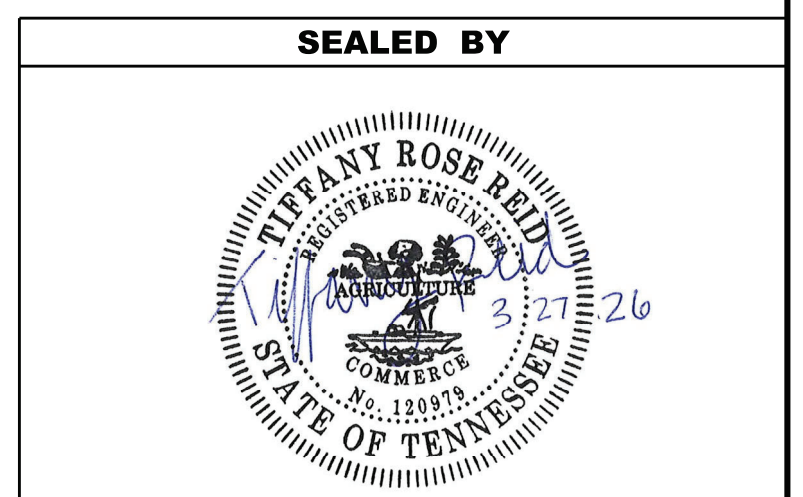
1. IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 0.75 INCH AND NOT EXCEEDING 2 INCHES.
  - a. WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.
2. IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 2 INCHES AND NOT EXCEEDING 6 INCHES:
  - a. SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:
    - (1) WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.
    - (2) WHERE POSTED SPEEDS ARE LESS THAN 50 MPH THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.
3. IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 6 INCHES:
  - a. SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:
    - (1) WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.
    - (2) WHERE POSTED SPEEDS ARE LESS THAN 50 MPH THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.
  - b. ELIMINATE VERTICAL OFFSET BY CONSTRUCTING A STONE WEDGE OR GRADING TO A 4:1 SLOPE, OR FLATTER, OR USE PORTABLE BARRIER RAIL.

THE CONTRACTOR SHALL SCHEDULE THE WORK SO AS TO MINIMIZE THE TIME TRAFFIC IS EXPOSED TO AN ELEVATION DIFFERENCE. ONCE THE CONTRACTOR BEGINS AN ACTIVITY THAT CREATES AN ELEVATION DIFFERENCE WITHIN 8 FEET OF A TRAFFIC LANE, THE ACTIVITY SHALL BE PURSUED AS A CONTINUOUS OPERATION UNTIL THE ELEVATION DIFFERENCE IS ELIMINATED.

## C. IF THE DIFFERENCE IN ELEVATION IS FARTHER THAN 8 FEET FROM THE NEAREST TRAFFIC LANE BUT NOT MORE THAN 30 FEET FROM THE NEAREST TRAFFIC LANE:

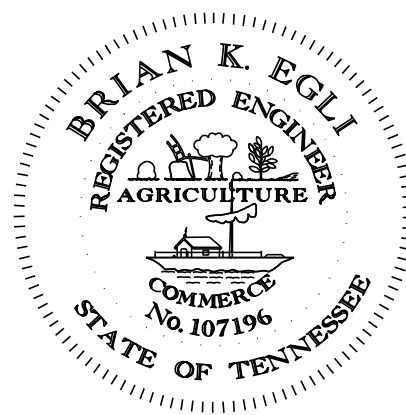
- SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:
1. WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.
  2. WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

THE CONTRACTOR SHALL SCHEDULE THE WORK SO AS TO MINIMIZE THE TIME TRAFFIC IS EXPOSED TO AN ELEVATION DIFFERENCE. ONCE THE CONTRACTOR BEGINS AN ACTIVITY THAT CREATES AN ELEVATION DIFFERENCE, THE ACTIVITY SHALL BE PURSUED AS A CONTINUOUS OPERATION UNTIL THE ELEVATION DIFFERENCE IS ELIMINATED.



**STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION**

**PAVEMENT EDGE  
DROP-OFF NOTES  
FOR  
TRAFFIC CONTROL**



THIS DOCUMENT HAS BEEN DIGITALLY SIGNED AND SEALED BY:

**BRIAN KENNETH EGLI**

**2026.03.26 09:39:48 -05'00'**

PRINTED COPIES OF THIS DOCUMENT ARE NOT CONSIDERED SIGNED AND SEALED. THE SIGNATURE MUST BE VERIFIED ON THE ELECTRONIC DOCUMENTS.

TENNESSEE DEPARTMENT OF TRANSPORTATION  
505 DEADERICK STREET, SUITE 1200  
NASHVILLE, TN 37243  
BRIAN K. EGLI, P.E. NO. 107196

THE ABOVE NAMED PROFESSIONAL ENGINEER SHALL BE RESPONSIBLE FOR THE FOLLOWING SHEETS IN ACCORDANCE OF TENN. CODE ANN. §62-2-306.

**SHEET NAME** **SHEET NO.**

SIGNATURE SHEET	BRIDGE-SIGN 1
INDEX OF DRAWINGS	B-1
BRIDGE TABULATION AND ESTIMATED QUANTITIES,	
REPAIRS NOTES AND DETAILS	B-2
FULL AND PARTIAL REPAIRS NOTES	B-3
PLAN VIEW REPAIRS LOCATION	B-4
PHASE CONSTRUCTION	B-5
PLAN VIEW REPAIRS LOCATION	B-6
PHASE CONSTRUCTION	B-7

\$\$\$\$SYTIME\$\$\$\$  
\$\$\$\$DCNPLC\$\$\$\$  
\$\$\$\$SYTIME\$\$\$\$  
\$\$\$\$DCNPLC\$\$\$\$

YEAR	PROJECT NO.	SHEET NO.
2026	58S002-M3-007	BRIDGE-SIGN 1

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

SIGNATURE  
SHEET

PROJECT NO.	YEAR	SHEET NO.	
58S002-M3-007	2026	B-1	
REVISIONS			
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INDEX OF DRAWINGS

DWG. NO.

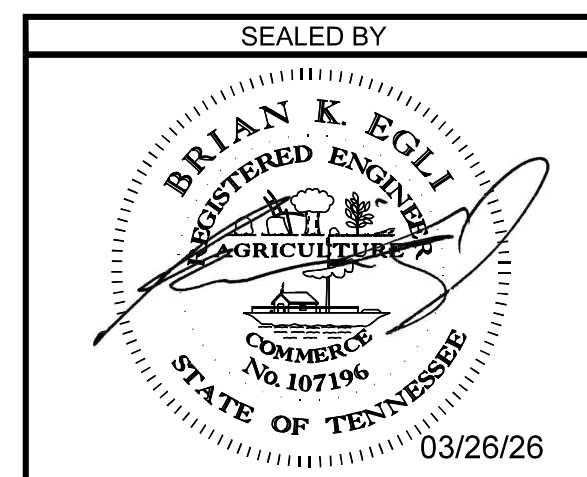
LAST  
REV. DATE

SIGNATURE SHEET	BRIDGE-SIGN 1
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AT BRIDGE ENDS	STD-1-5



STATE OF TENNESSEE  
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OVER I-24 E.B.L.  
MARION COUNTY  
2026

PIN NO.: 134114.00  
DESIGN BY: ASHRAF.ARMANIOS DATE: 11/13/2025  
DRAWN BY: ASHRAF.ARMANIOS DATE: 11/13/2025  
SUPERVISED BY: DARRELL.PALMORE DATE: 11/13/2025  
CHECKED BY: KEVIN.MARTINKO DATE: 02/25/2026

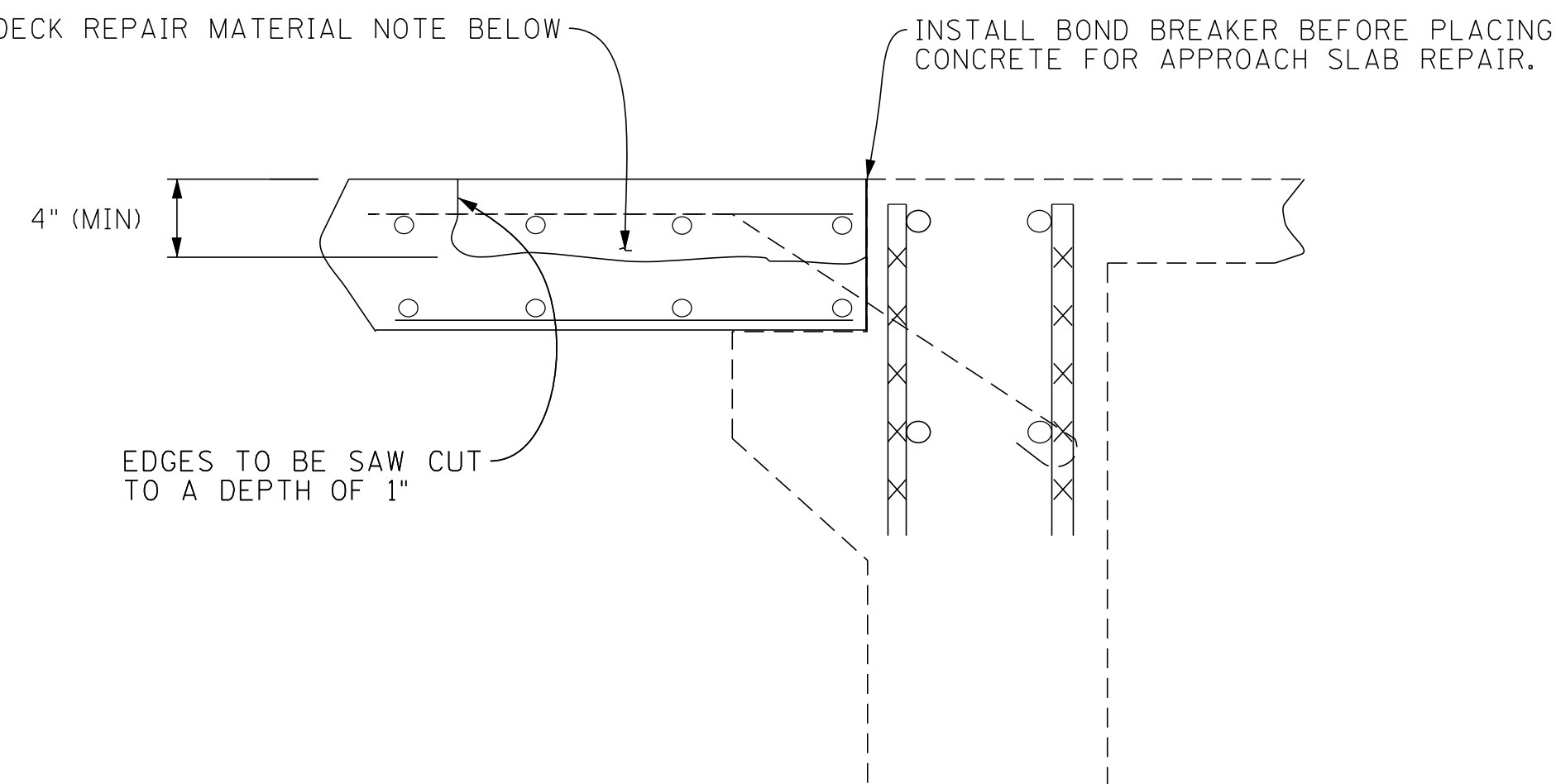
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58S002-M3-007	2026	B-2	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
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TABULATION OF BRIDGE RELATED WORK AND ESTIMATED QUANTITIES							
LOCATION OF BRIDGE AND BRIDGE NUMBER	REFERENCE DRAWINGS TO BE PRINTED WITH CONTRACT DRAWINGS	TYPE OF WORK	604-10.53 CONCRETE REPAIRS (PARTIAL DEPTH OF APPROACH PAVEMENT) S.Y.	604-10.50 BRIDGE DECK REPAIRS (PARTIAL DEPTH OF SLAB) S.Y. *	604-10.30 BRIDGE DECK REPAIRS (FULL DEPTH OF SLAB) S.Y. **	617-01 BRIDGE DECK SEALANT S.Y.	307-03.08 ASPHALT CONCRETE MIX (PG76-22) (BPMB-HM) GRADING "B-M2" TON
58-SR002-1.4 OVER I-24 W.B.L. (58100240071)	M-307-63 M-307-66 STD-1-5	CONCRETE REPAIRS (PARTIAL DEPTH OF APPROACH PAVEMENT)	5	70	5	729	84
58-0A550-0.01 OVER I-24 E.B.L. (58100240071)	M-307-76 M-307-79 STD-1-5	CONCRETE REPAIRS (PARTIAL DEPTH OF APPROACH PAVEMENT)	10	180	40	908	105
TOTAL			15	250	45	1637	189

\* DENOTES: PARTIAL DEPTH DECK REPAIRS AREAS WILL BE LOCATED AFTER THE EXISTING ASPHALT IS REMOVED.

\*\* DENOTES: FULL DEPTH DECK REPAIRS AREAS WILL BE LOCATED AFTER THE EXISTING ASPHALT IS REMOVED.

SEE BRIDGE DECK REPAIR MATERIAL NOTE BELOW

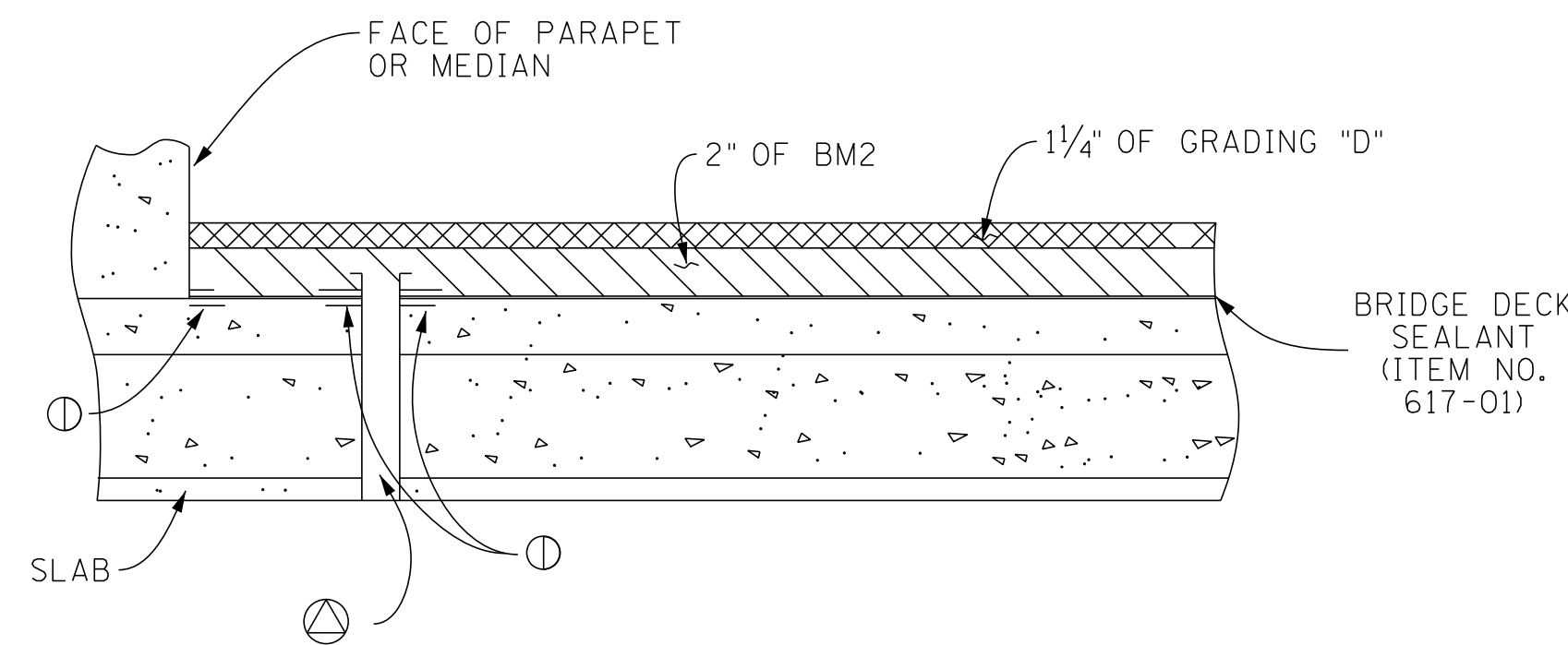


**CONCRETE APPROACH PAVEMENT REPAIR DETAILS:**

NOTE: REMOVE CONCRETE TO A DEPTH OF 3/4" MINIMUM BELOW EXISTING REINFORCING STEEL OR TO SOUND CONCRETE. CONCRETE REMOVAL SHOULD BE TO A MINIMUM DEPTH OF 4". DEPTH FOR CONCRETE REMOVAL SHOULD NOT BE MORE THAN 6".

NOTE: COST OF SAW CUTTING, CONCRETE REMOVAL, COMPLETELY CLEANING EXPOSED REINFORCING STEEL, LABOR, AND ANY MISCELLANEOUS MATERIALS NECESSARY TO COMPLETE THE REPAIRS AS SHOWN SHALL BE INCLUDED IN ITEM NO. 604-10.53 CONCRETE REPAIR S.Y.

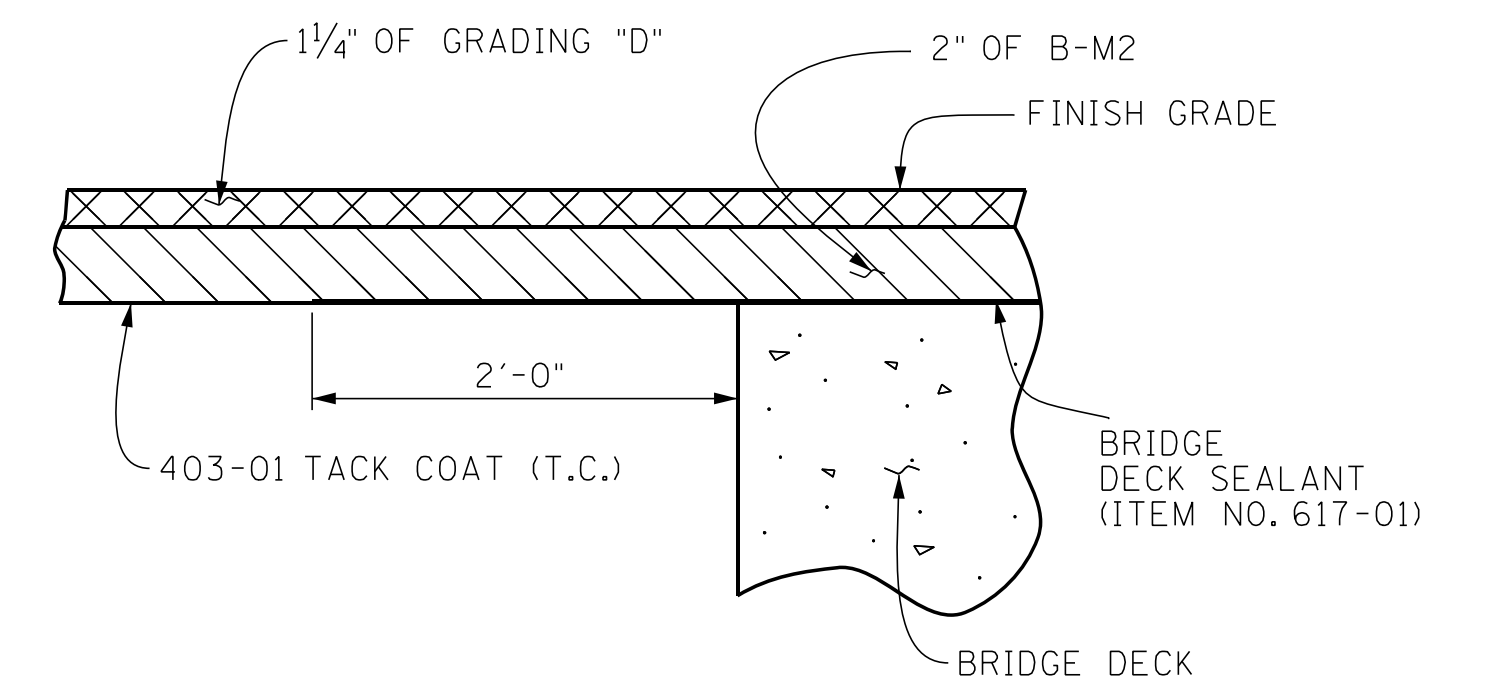
NOTE: ITEM NO. 604-10.53, CONCRETE REPAIR (PARTIAL DEPTH OF APPROACH PVMT.) MAY BE INCREASED, DECREASED OR ELIMINATED AS DIRECTED BY THE ENGINEER.



**TYPICAL AT FACE OF EXISTING PARAPET OR MEDIAN**

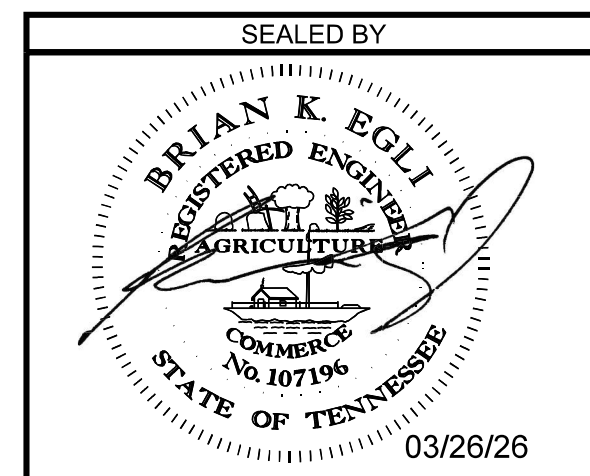
⊙ MASTIC AS RECOMMENDED BY MANUFACTURER OF MEMBRANE. SEE STD. SPEC. ART. 906.04

⊙ 1/2" DIAMETER HOLE TO BE DRILLED BY THE CONTRACTOR NEAR FACE OF EXISTING CONCRETE PARAPETS OR MEDIAN AT LOW POINTS, COST TO BE INCLUDED IN ITEM NO. 617-01



**BRIDGE DECK SEALANT DETAIL**

NOTE: ON BRIDGE DECK WHERE THERE IS A BITUMINOUS JOINT OR WITH NO EXPANSION JOINT, MEMBRANE SHALL EXTEND 2'-0" OVER THE END OF THE BRIDGE DECK. IF THE DECK HAS A STEEL EXPANSION JOINT, THE MEMBRANE SHALL STOP AT THE STEEL EXPANSION JOINT.



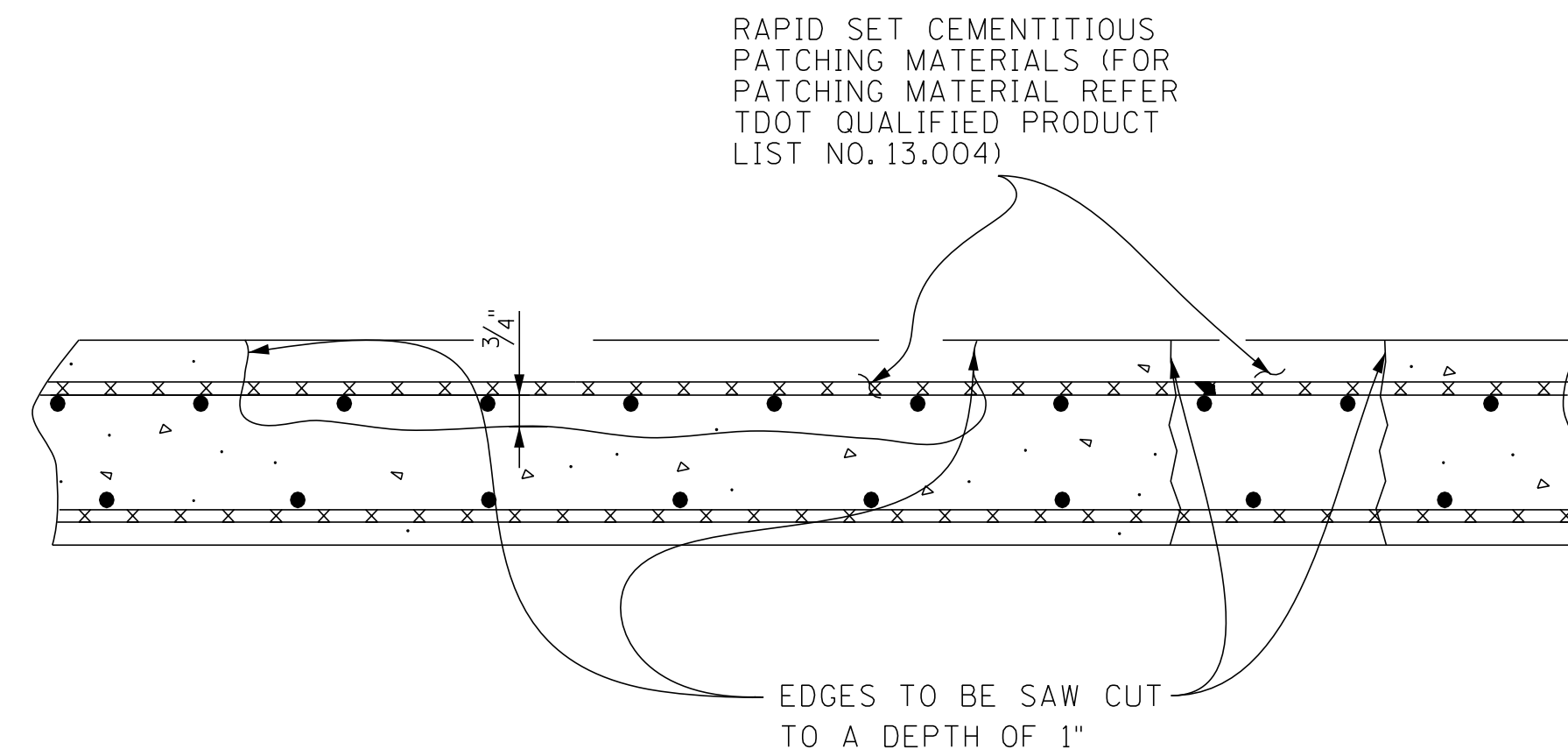
STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

BRIDGE TABULATION AND  
ESTIMATED QUANTITIES,  
REPAIRS NOTES AND DETAIL  
BR.58-SR2-1.40 (58100240071)  
OVER I-24 W.B.L.  
BR.58-0A550-0.01 (58100240007)  
OVER I-24 E.B.L.  
MARION COUNTY  
2026

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\$\$\$\$\$ DGNISPEC\$\$\$\$\$

PIN NO.: 134114.00  
DESIGN BY: ASHRAF.ARMANIOS DATE: 11/13/2025  
DRAWN BY: ASHRAF.ARMANIOS DATE: 11/13/2025  
SUPERVISED BY: DARRELL.PALMORE DATE: 11/13/2025  
CHECKED BY: KEVIN.MARTINKO DATE: 02/25/2026

PROJECT NO.	YEAR	SHEET NO.	
58S002-M3-007	2026	B-3	
REVISIONS			
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SKETCH SHOWING DECK REPAIR

NOTE: PARTIAL DEPTH DECK REPAIR ONLY REMOVE CONCRETE IN ALL DELAMINATED AREAS TO A DEPTH OF 3/4" BELOW THE TOP BAR OF THE TOP MAT OF REINFORCING STEEL.

NOTE: PARTIAL AND FULL DEPTH DECK REPAIRS ALL REINFORCING STEEL IN THE AREAS OF THE DECK REPAIRS SHALL BE COMPLETELY CLEANED. CLEANING SHALL BE DONE PRIOR TO PLACING NEW CONCRETE. AREAS OF CONCRETE REMOVAL SHALL BE DESIGNATED BY PERSONNEL FROM THE HEADQUARTERS, BRIDGE INSPECTION AND REPAIR OFFICE. INSPECTIONS TO DETERMINE AREAS OF DECK REPAIR SHALL BE SCHEDULED WITH THE BRIDGE REPAIR OFFICE AT LEAST (3) DAYS IN ADVANCE.

THE COST OF CONCRETE, CONCRETE REMOVAL, LABOR, CLEANING REBAR AND ANY MISCELLANEOUS MATERIAL REQUIRED SHALL BE PAID FOR UNDER ITEM NO. 604-10.50 BRIDGE DECK REPAIR (PARTIAL DEPTH OF SLAB), S.Y. AND ITEM NO. 604-10.30 BRIDGE DECK REPAIR (FULL DEPTH) S.Y..

NOTE: ITEM NOS. 604-10.50, BRIDGE DECK REPAIR (PARTIAL DEPTH OF SLAB) AND 604-10.30 BRIDGE DECK REPAIR (FULL DEPTH) SHALL BE ITEMS BID ON SHALL BE BID THIS MAY BE INCREASED, DECREASED OR ELIMINATED AS APPROVED BY THE ENGINEER.

NOTE: AREAS OF PARTIAL DEPTH REPAIR SURFACE OF AGGREGATE OR CONCRETE MUST BE SATURATED WITH CLEAN WATER, GIVE IT TIME TO BECOME DAMP (NOT WET) BLAST ANY EXCESSIVE WATER, PATCHED AREA TO BE SATURATED SURFACE DRY UNLESS OTHERWISE NOTED IN MANUFACTURER'S SPECIFICATION.

NOTE: TRAFFIC CONTROL SHALL BE PROVIDED ON ROADWAY UNDERPASS DURING PARTIAL AND FULL DEPTH DECK REPAIR OF OVERHEAD BRIDGE.

NOTE: PRIOR TO THE PLACEMENT OF THE BRIDGE DECK SEAL MEMBRANE, THE BRIDGE SHALL BE CLEANED OF ALL LOOSE DEBRIS AND EXCESS DIRT. THE BRIDGE DECK SHALL ALSO BE CLEANED OF ALL OIL, DIESEL OR ANY OTHER PETROLEUM PRODUCT SPILLS AND DRY ACCORDING TO THE MANUFACTURER'S SPECIFICATIONS.

THE WATERPROOFING MEMBRANE SHALL BE APPLIED ACCORDING TO THE MANUFACTURER'S SPECIFICATIONS. THE COST OF ALL THE MATERIALS NECESSARY FOR THE WATERPROOFING APPLICATION (WATERPROOFING MEMBRANE, PRIMER, MASTIC, ETC.) AND LABOR SHALL BE PAID FOR UNDER ITEM NO. 617-01 BRIDGE DECK SEALANT S.Y.

NOTE: BRIDGE DECK SEALANT SHALL BE APPLIED DIRECTLY TO THE CONCRETE SURFACE OF THE BRIDGE DECK AS NOTED IN THE BRIDGE DECK SEALANT DETAIL.

NOTE: FORMS AND FALSEWORK : ALL CONCRETE FORMS AND FALSEWORK SHALL BE REMOVED AFTER REPAIRS ARE COMPLETED. COST OF REMOVAL SHALL BE INCLUDED IN ITEMS BID ON. THIS WORK SHALL BE COMPLETED BEFORE FINAL PAYMENT IS APPROVED.

POWER DRIVEN HAND TOOLS:

POWER DRIVEN HAND TOOLS USED FOR THE REMOVAL OF UNSOUND CONCRETE IN MAKING PARTIAL AND FULL DEPTH REPAIRS ARE SUBJECT TO THE FOLLOWING RESTRICTIONS:

(1) PARTIAL DEPTH REPAIRS; PNEUMATIC HAMMERS HEAVIER THAN NOMINAL 60 LB. CLASS SHALL NOT BE USED. ALSO TRAFFIC CONTROL SHALL BE SET UP DURING PARTIAL DEPTH REPAIRS OVER TRAFFIC.

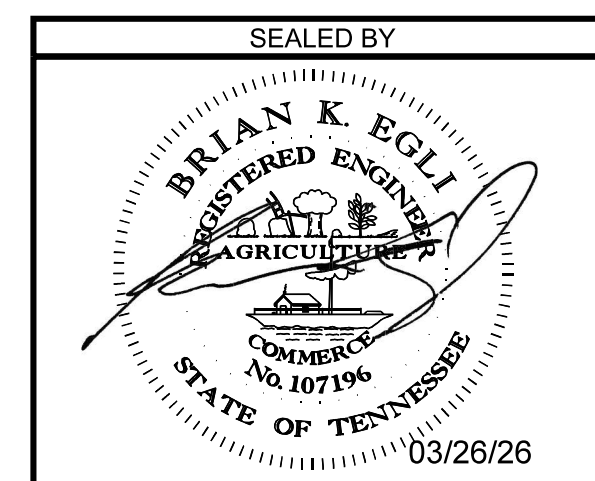
(2) CHIPPING HAMMERS OF THE 15 LB. CLASS SHALL BE USED TO REMOVE CONCRETE FROM BENEATH ANY REINFORCING STEEL.

BRIDGE DECK REPAIR MATERIAL

ALL REPAIRS SHALL USE AN EXTENDED QUICK SET PATCHING MATERIAL FROM THE DOT QUALIFIED PRODUCTS LIST: 13.004-RAPID SET CEMENTITIOUS PATCHING MATERIALS. PATCHING MATERIAL SHALL BE APPLIED ACCORDING TO MANUFACTURER'S SPECIFICATIONS. MIX MUST MEET 3000 P.S.I. BEFORE OPENING TO TRAFFIC.

OPTIONAL BRIDGE DECK REPAIR MATERIAL

IF OVERNIGHT LANE CLOSURES ARE PROVIDED FOR THIS PROJECT AND THE LANE CLOSURES ARE SUCH THAT TRAFFIC WILL BE KEPT OFF THE BRIDGE DECK WHERE DECK REPAIRS WILL BE MADE. THEN HIGH EARLY STRENGTH CONCRETE SHALL BE USED WITH A COMPRESSIVE STRENGTH OF 3,000 P.S.I. AT 18 HOURS. THE CONTRACTOR SHALL PROVIDE PROOF PRIOR TO BEGINNING WORK THAT PROPOSED CONCRETE MIX WILL OBTAIN REQUIRED PROPERTIES. PROOF SHALL BE PROVIDED BY AN INDEPENDENT TESTING COMPANY AND SUBMITTED TO THE MATERIALS AND TESTS DIVISION OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION FOR APPROVAL. TRAFFIC SHALL NOT BE ALLOWED ON REPAIRED AREAS UNTIL THE CONCRETE HAS BEEN IN PLACE A MINIMUM OF 18 HOURS.

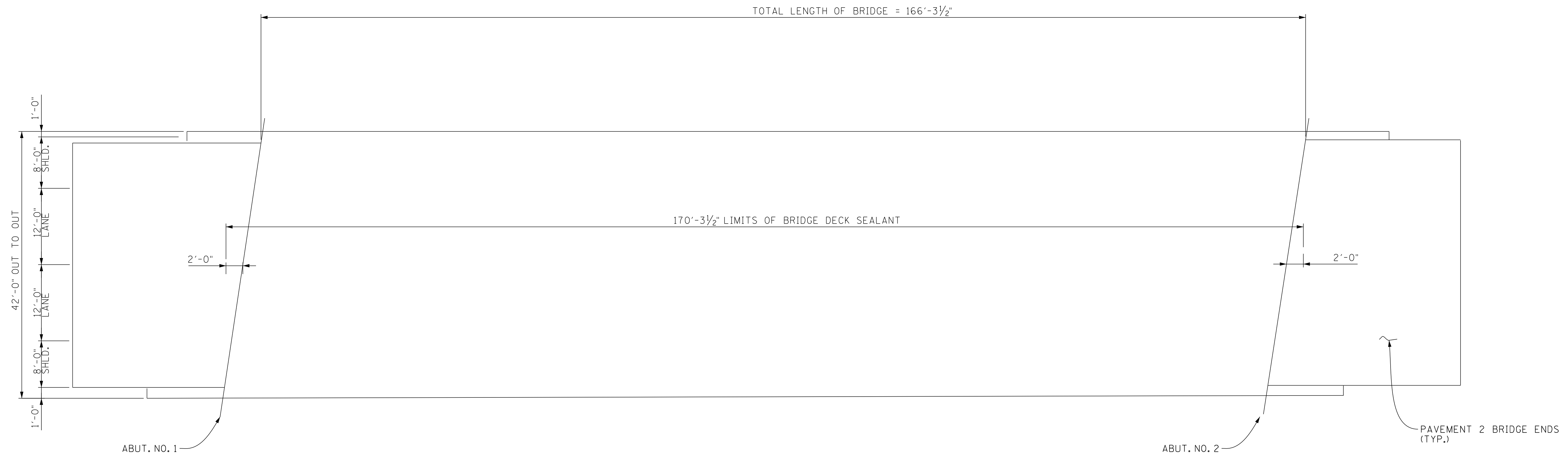


STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
BRIDGE FULL AND  
PARTIAL ON SLAB  
REPAIRS NOTES AND DETAILS  
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OVER I-24 W.B.L.  
BR.58-0A550-0.01 (58100240007)  
OVER I-24 E.B.L.  
MARION COUNTY  
2026

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PIN NO.: 134114.00  
DESIGN BY: ASHRAF.ARMANIOS DATE: 11/13/2025  
DRAWN BY: DARRELL.PALMORE DATE: 11/13/2025  
SUPERVISED BY: KEVIN.MARTINKO DATE: 02/25/2026  
CHECKED BY:

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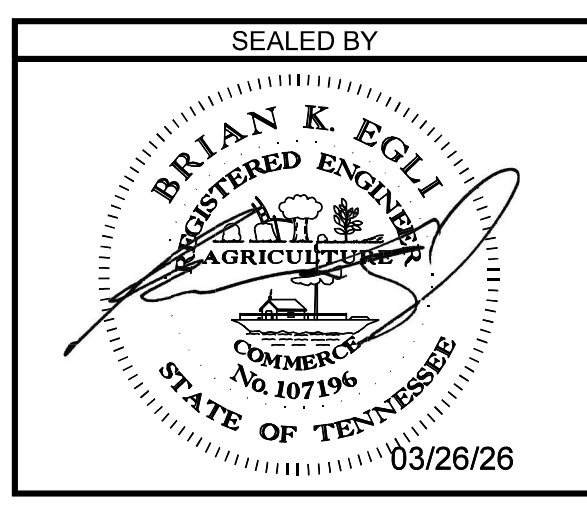


**PLAN VIEW**

58-SR2-1.40

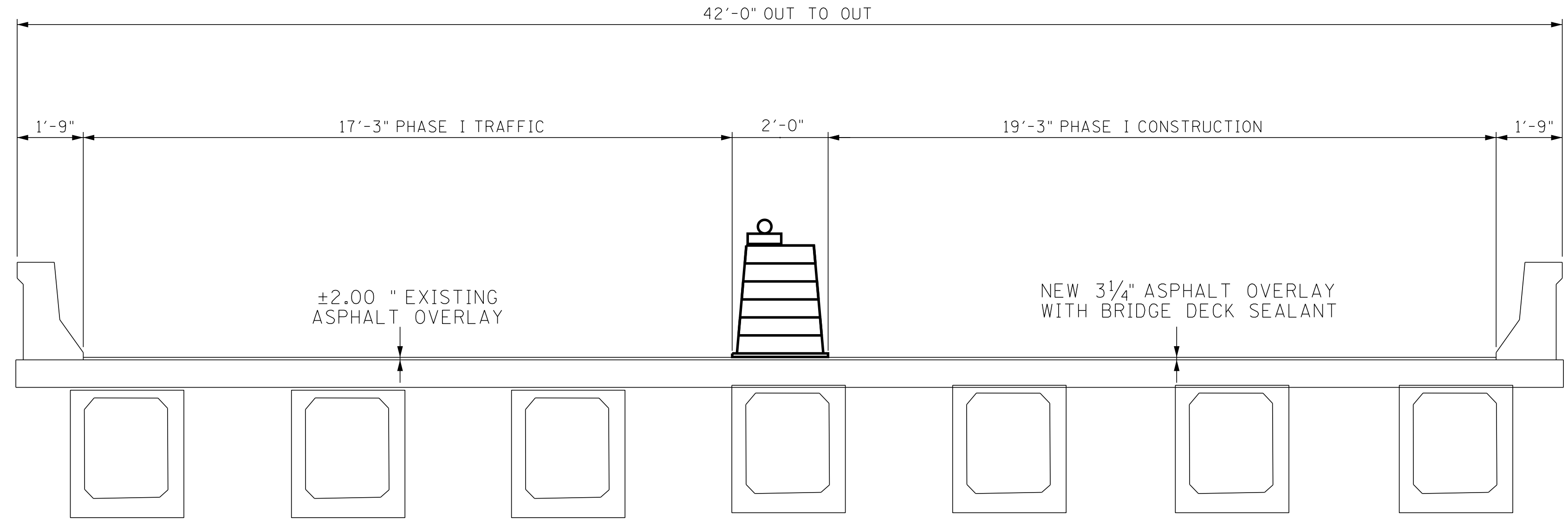
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PIN NO.: 134114.00  
 DESIGN BY: \_\_\_\_\_ DATE: \_\_\_\_\_  
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 CHECKED BY: KEVIN.MARTINKO DATE: 02/25/2026

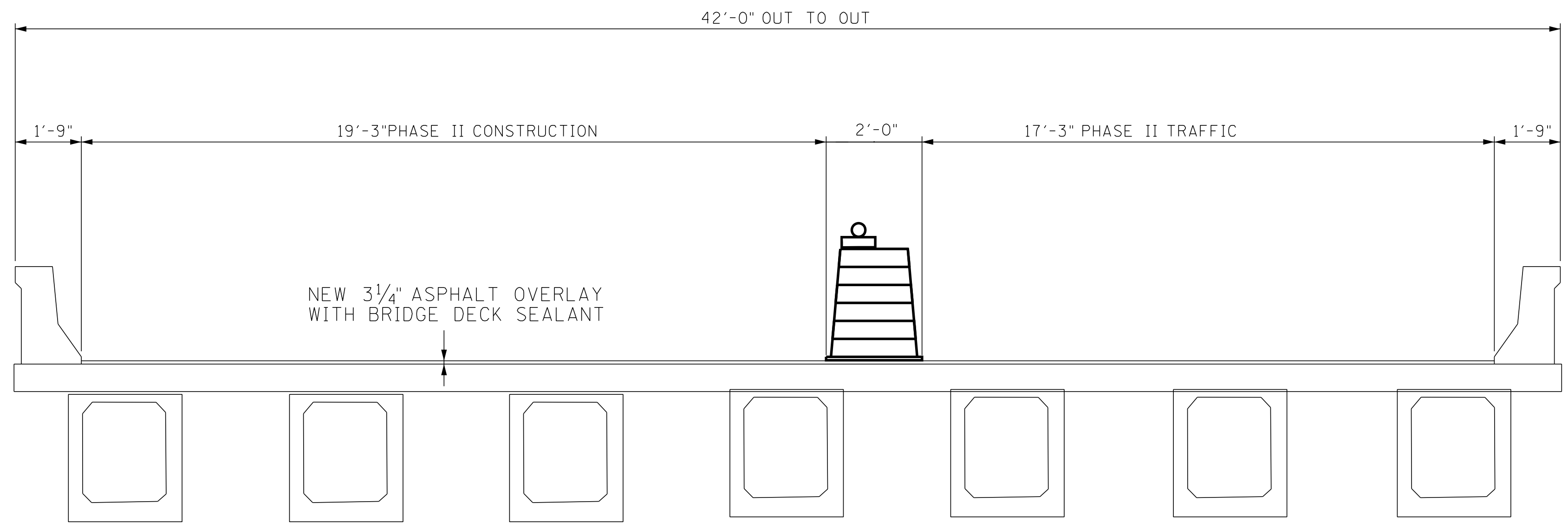


STATE OF TENNESSEE  
 DEPARTMENT OF TRANSPORTATION  
 PLAN VIEW  
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 (58100240071)  
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 I-24 W.B.L.  
 MARION COUNTY  
 2026

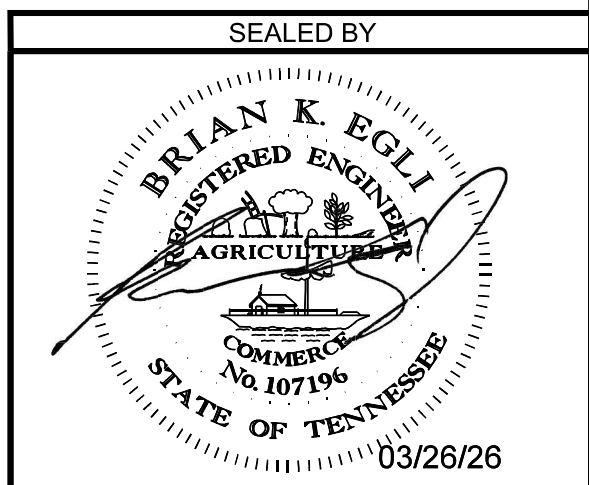
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PHASE I CONSTRUCTION  
(LOOKING AHEAD ON THE SURVEY)



PHASE II CONSTRUCTION  
(LOOKING AHEAD ON THE SURVEY)

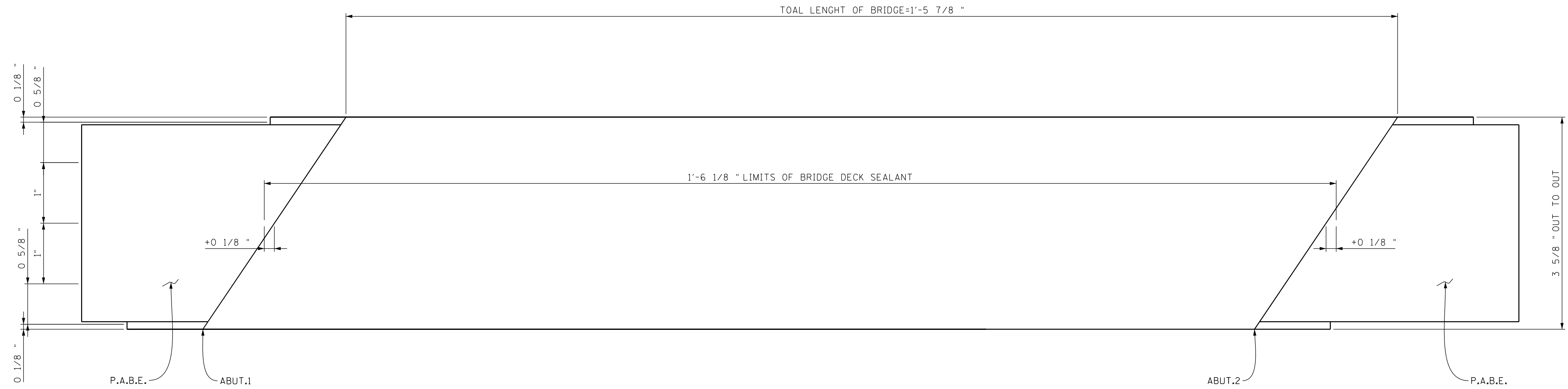


STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

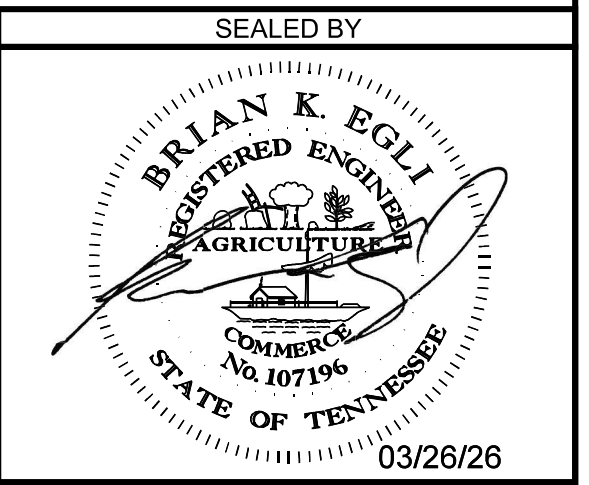
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(58100240071)  
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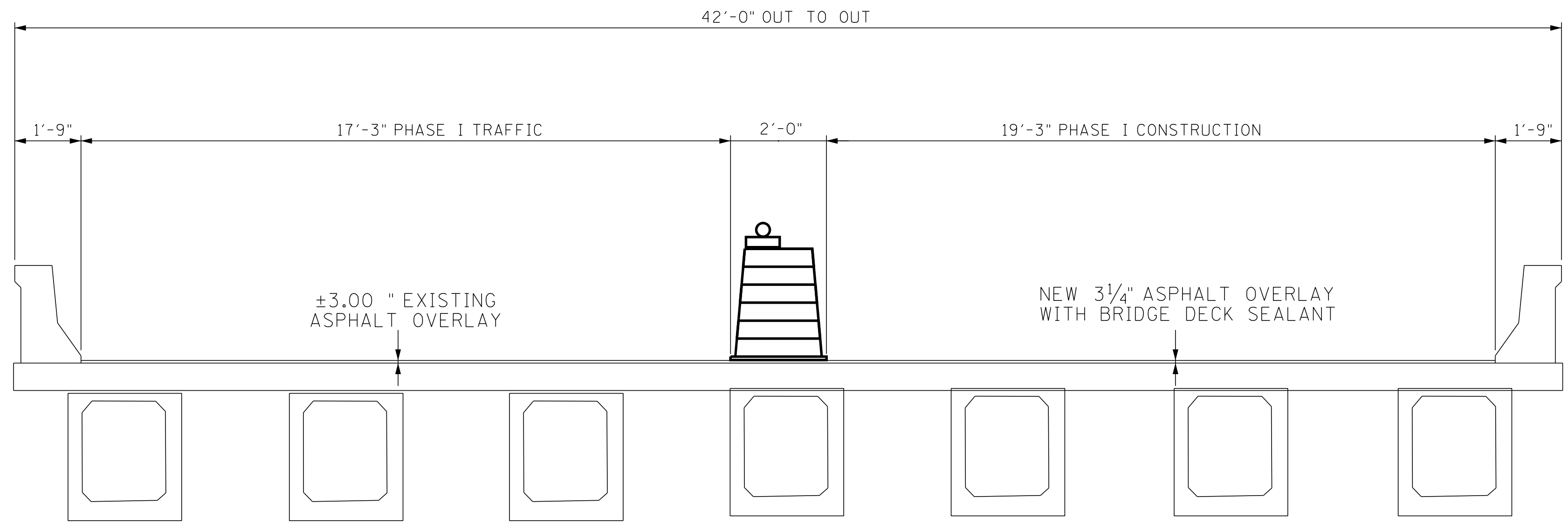
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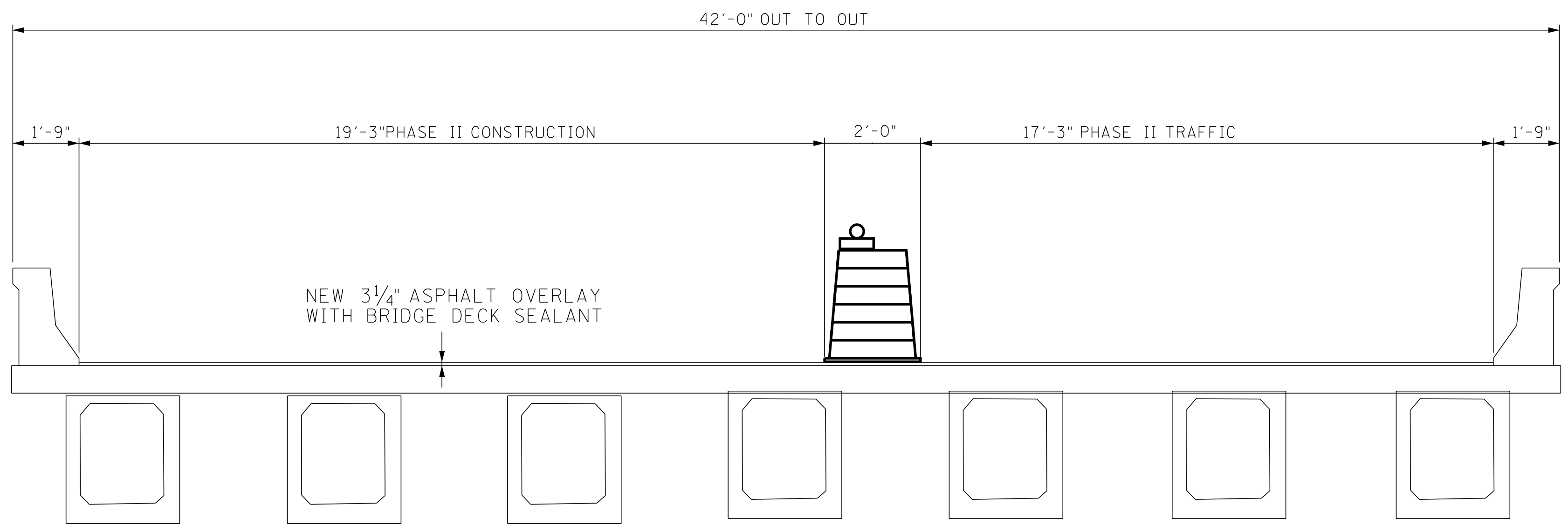
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DEPARTMENT OF TRANSPORTATION  
PLAN VIEW  
REPAIR LOCATIONS  
BR.58-0A550-0.01  
(58100240007)  
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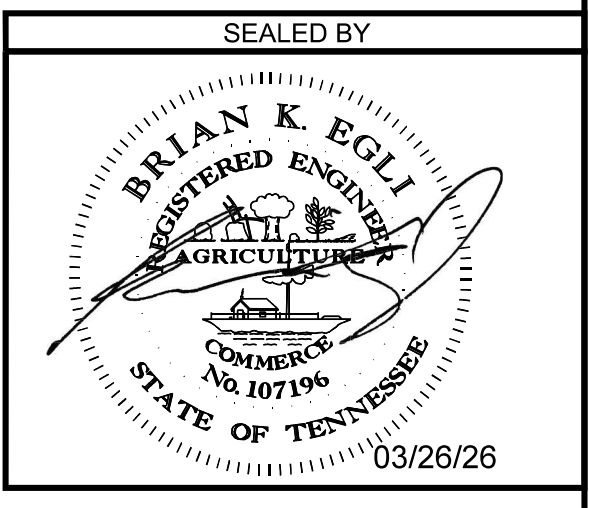
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PHASE I CONSTRUCTION  
(LOOKING AHEAD ON THE SURVEY)



PHASE II CONSTRUCTION  
(LOOKING AHEAD ON THE SURVEY)



STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
PHASING SKETCH  
CONSTRUCTION AND  
TRAFFIC CONTROL  
BR.58-0A550-0.01  
(58100240007)  
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I-24 E.B.L.  
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2026

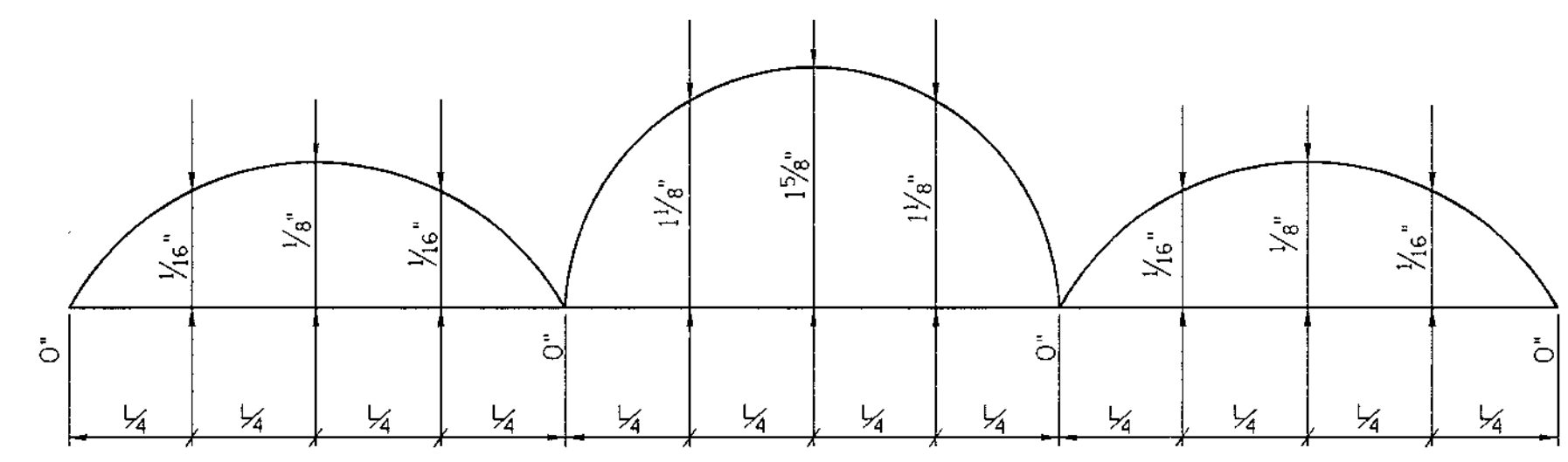
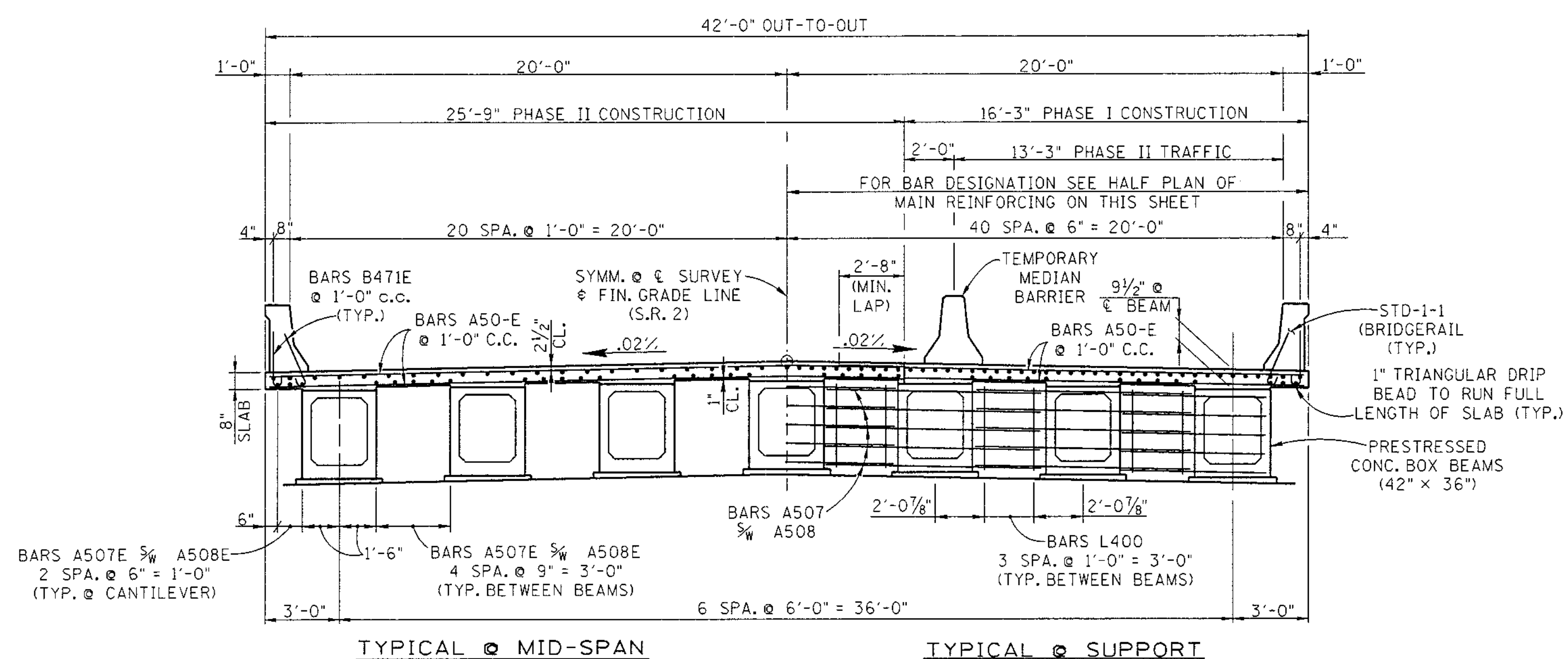
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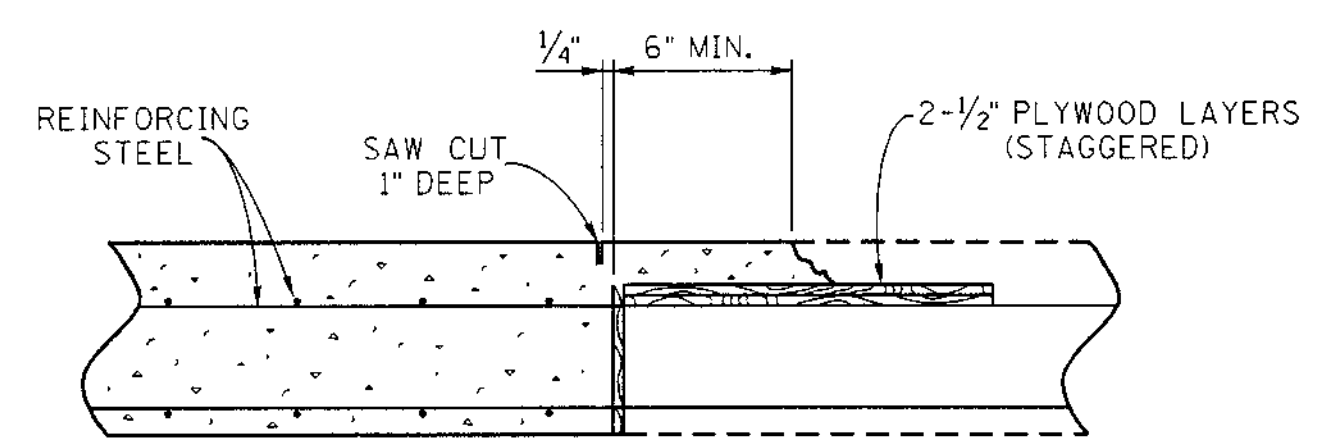
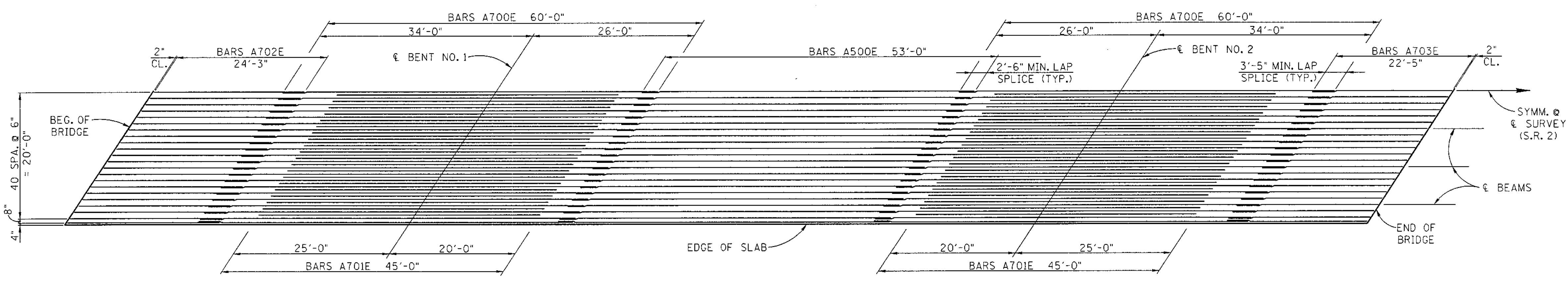


CONST. NO. 58004-1205-94			
PROJECT NO.	YEAR	SHEET NO.	
BR-STP-2(20)	1995		
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

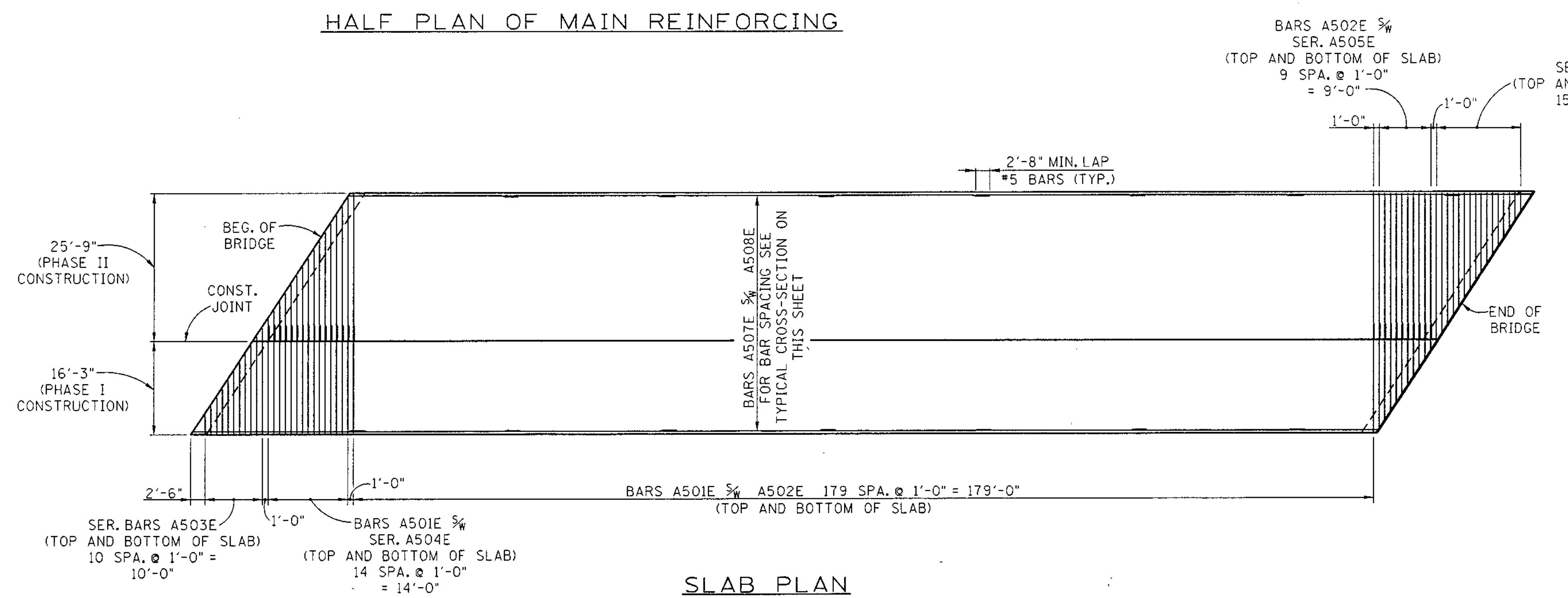


**DEAD LOAD CORRECTION CURVE**  
 THIS CURVE IS FOR DEAD LOAD SLAB AND ALL DEAD LOADS THAT ARE APPLIED AFTER THE SLAB IS IN PLACE AND SHOULD BE CORRECTED TO COMPENSATE FOR THE EFFECTS DUE TO VERTICAL CURVE.

DECK CONCRETE POURING SEQUENCE: SLAB CONSTRUCTION JOINTS MAY BE LOCATED AT THE CONTRACTOR'S OPTION SUBJECT TO THE FOLLOWING:  
 1) NO CONSTRUCTION JOINT MAY BE LOCATED CLOSER THAN 10 FEET OR FURTHER THAN 15 FEET FROM AN INTERIOR SUPPORT.  
 2) THE SLAB IN THE MIDDLE SECTION OF BOTH ADJACENT SPANS MUST BE POURED TO WITHIN AT LEAST 15 FEET OF THE SUPPORTS EITHER PRIOR TO OR CONCURRENTLY WITH THE SLAB OVER AN INTERIOR SUPPORT.



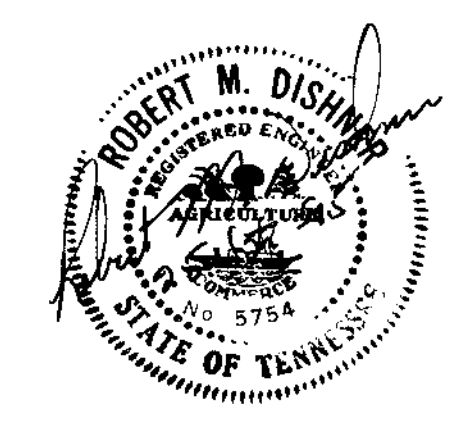
**GENERAL NOTES:**  
 NOTE: WHEN POURING SLAB, PROVISIONS SHALL BE MADE FOR SETTING REINFORCING STEEL FOR PARAPET. THE PARAPET SHALL NOT BE POURED UNTIL THE ENTIRE SLAB IS POURED AND CURED. ALSO, SEE STANDARD DRAWING STD-1-1.  
 SPECIAL NOTE FOR ANCHOR BOLTS AT BENTS: ANCHOR BOLT ASSEMBLIES AT BENTS SHALL BE IN ACCORDANCE WITH STANDARD DRAWING STD-6-1.  
 NOTE: THE CONTRACTOR IS SOLELY RESPONSIBLE FOR SUPPORTING THE BEAMS TO PREVENT DAMAGE DUE TO TWISTING OR OVERTURNING DURING ALL PHASES OF CONSTRUCTION.  
 NOTE: SUPPORT DIAPHRAGMS AT BENTS SHALL BE POURED CONCURRENTLY WITH THE DECK SLAB AND INCLUDED IN THE QUANTITY FOR ITEM 604-03.35.



**ESTIMATED QUANTITIES**

CLASS "L" CONCRETE (BRIDGE DECK) C.Y.	STEEL BAR REINFORCEMENT (BRIDGES) LB.	EPOXY-COATED REINFORCING STEEL (BRIDGES) LB.
245	1,646	53,574

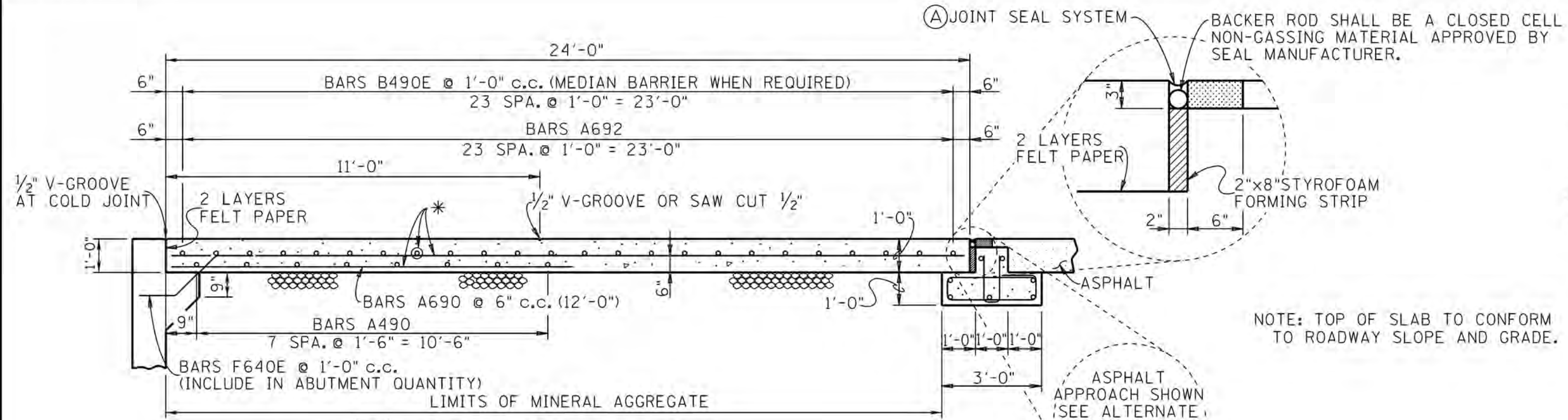
STATE OF TENNESSEE  
 DEPARTMENT OF TRANSPORTATION  
 BUREAU OF HIGHWAYS  
**BRIDGE NO. 2  
 SUPERSTRUCTURE  
 STATE ROUTE 2  
 OVER INTERSTATE 24  
 (EASTBOUND LANE)  
 STATION 28+60.00  
 MARION COUNTY  
 1995**



CORRECT *Edward P. Wasserman*  
 ENGINEER OF STRUCTURES

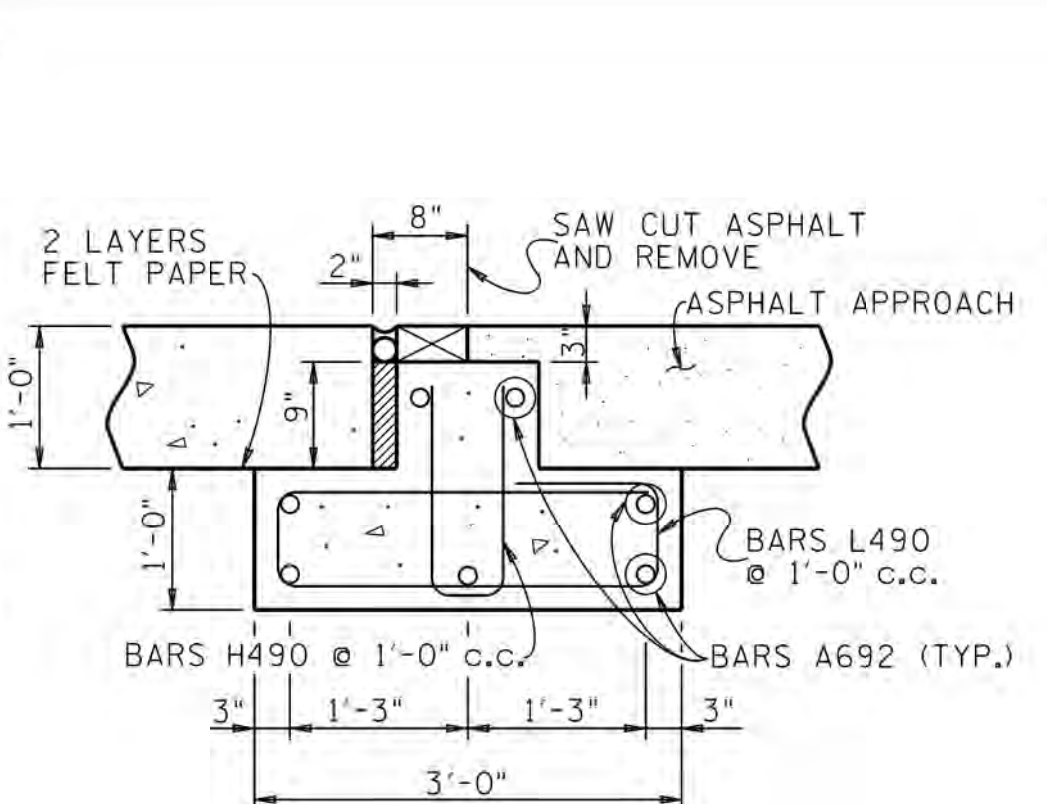
DESIGNED BY TERRY MACKIE DATE 08-94  
 DRAWN BY STEVEN STEELE DATE 08-94  
 SUPERVISED BY K.D.F./G.H. DATE 08-94  
 CHECKED BY DATE

**SLAB PLAN**



**NOTE:** TOP OF SLAB TO CONFORM TO ROADWAY SLOPE AND GRADE.

**NOTE:** WHEN BRIDGE END DRAINS ARE REQUIRED, ANY REINFORCING STEEL INTERFERING WITH BRIDGE END DRAIN SHALL BE CUT IN FIELD.



**NOTE:** TO BE USED ONLY WHEN ROADWAY PAVING IS NOT INCLUDED IN PROJECT.

**BILL OF STEEL**

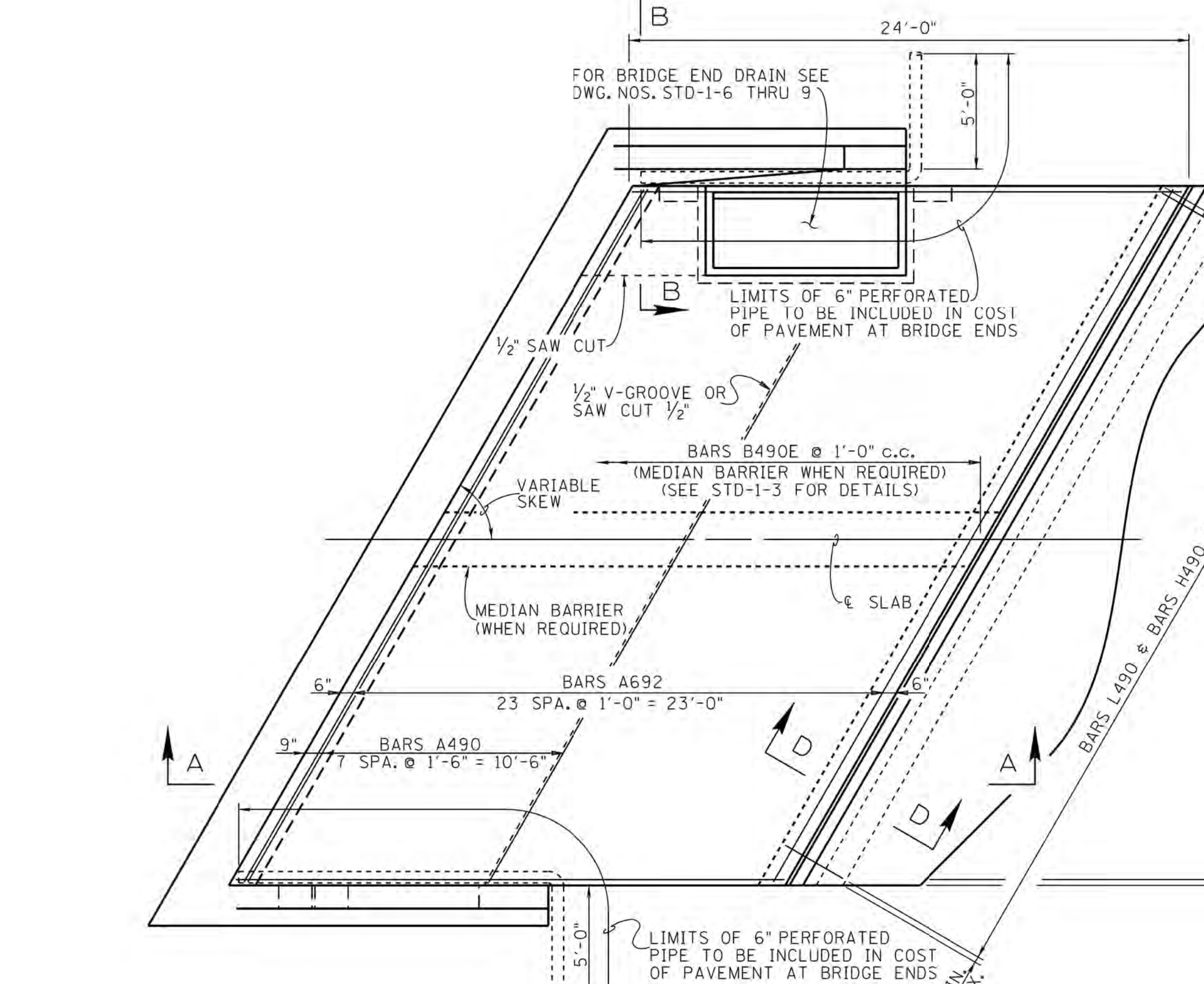
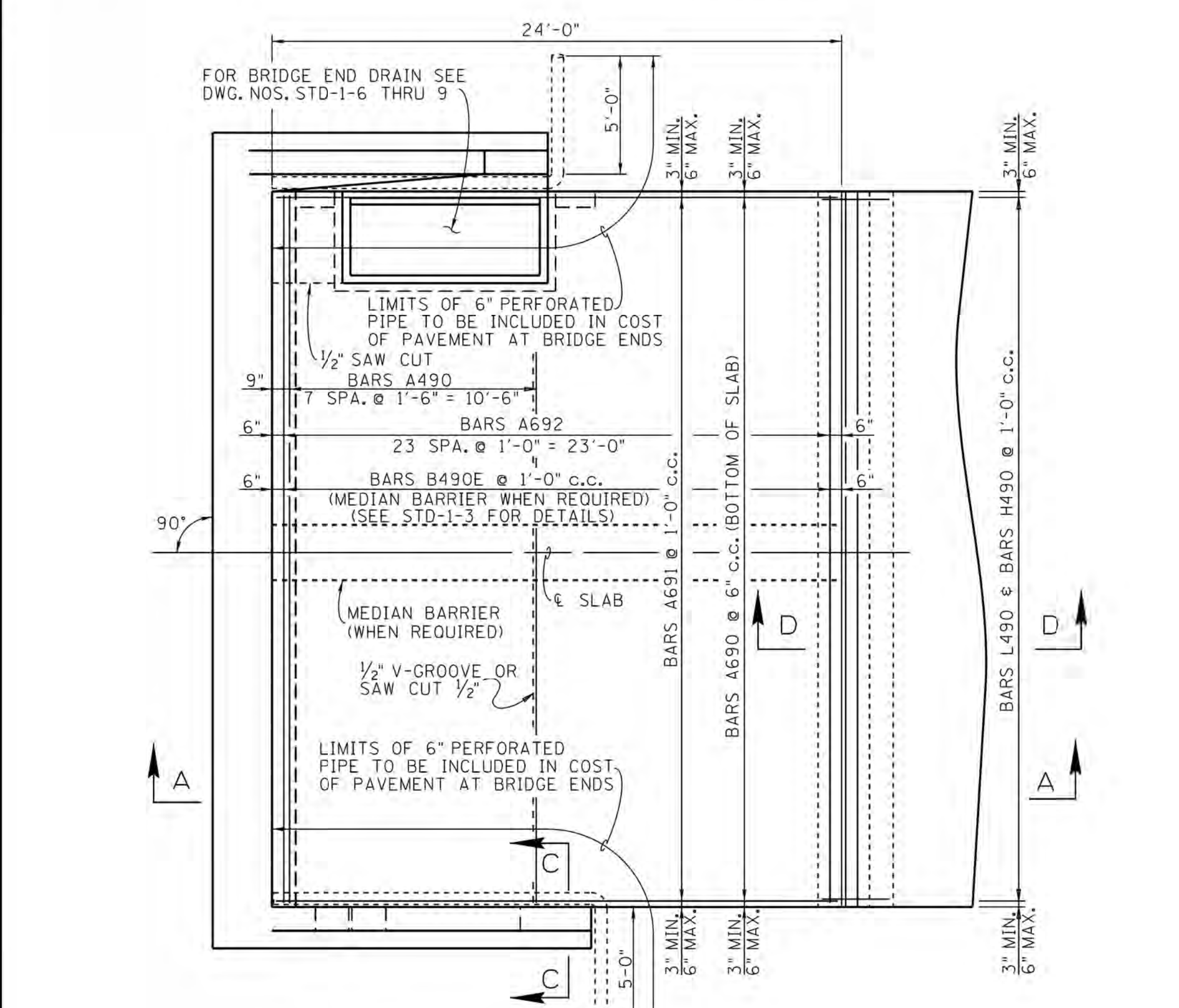
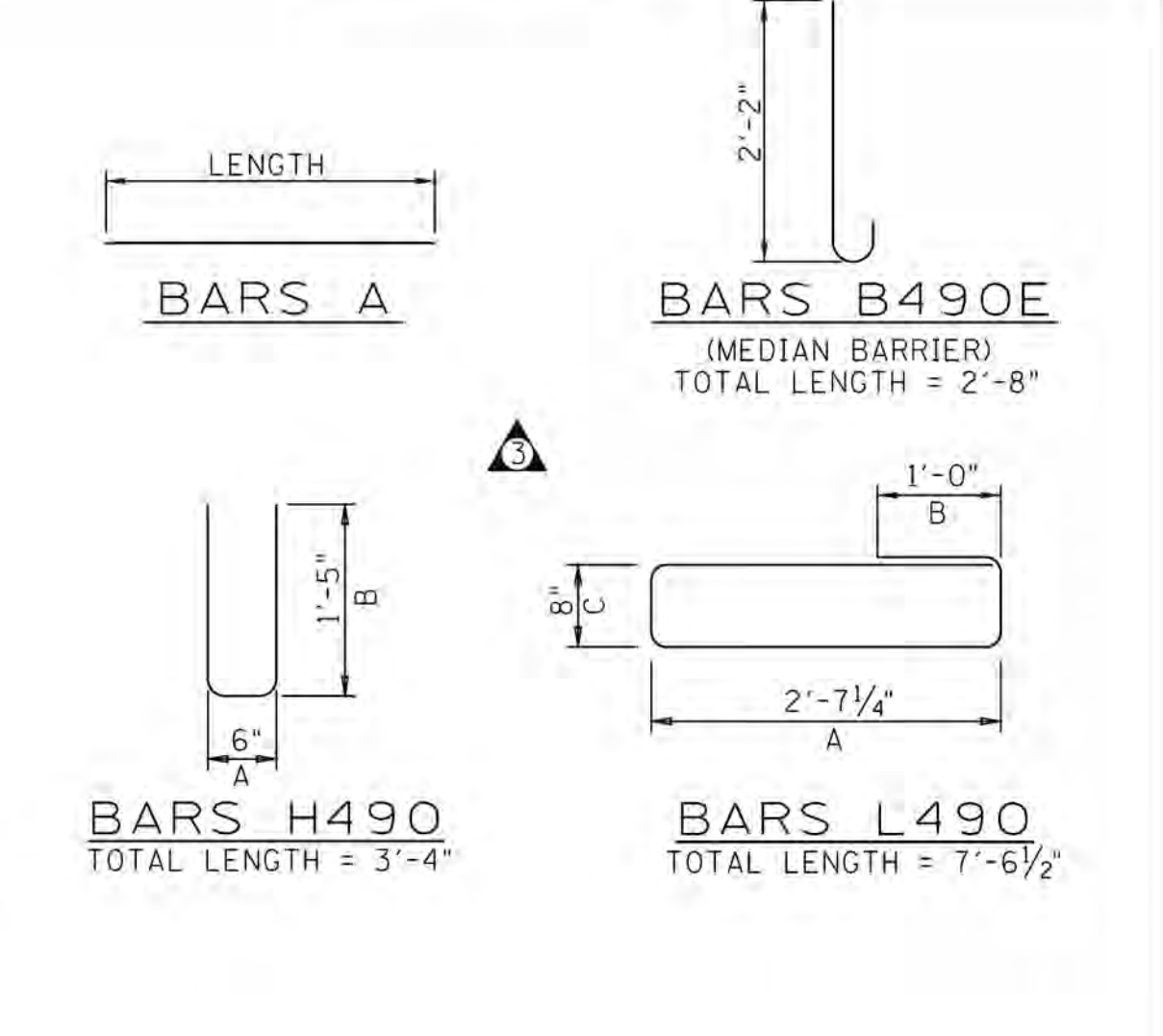
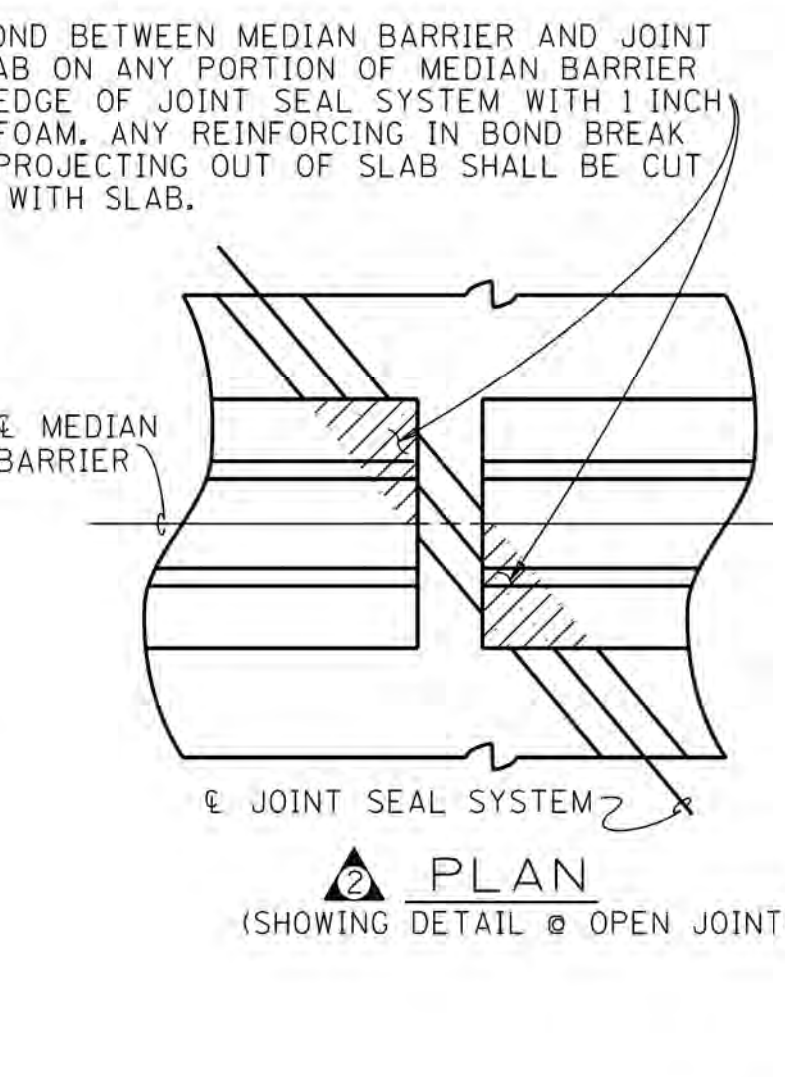
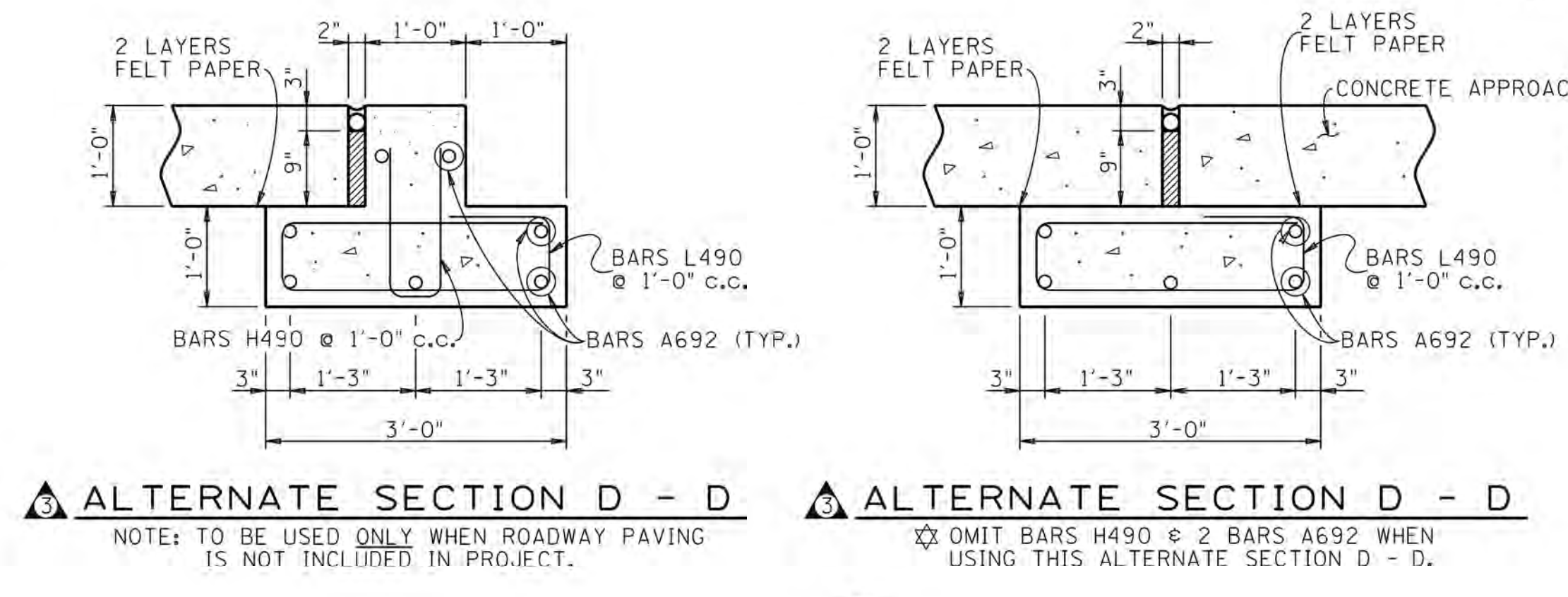
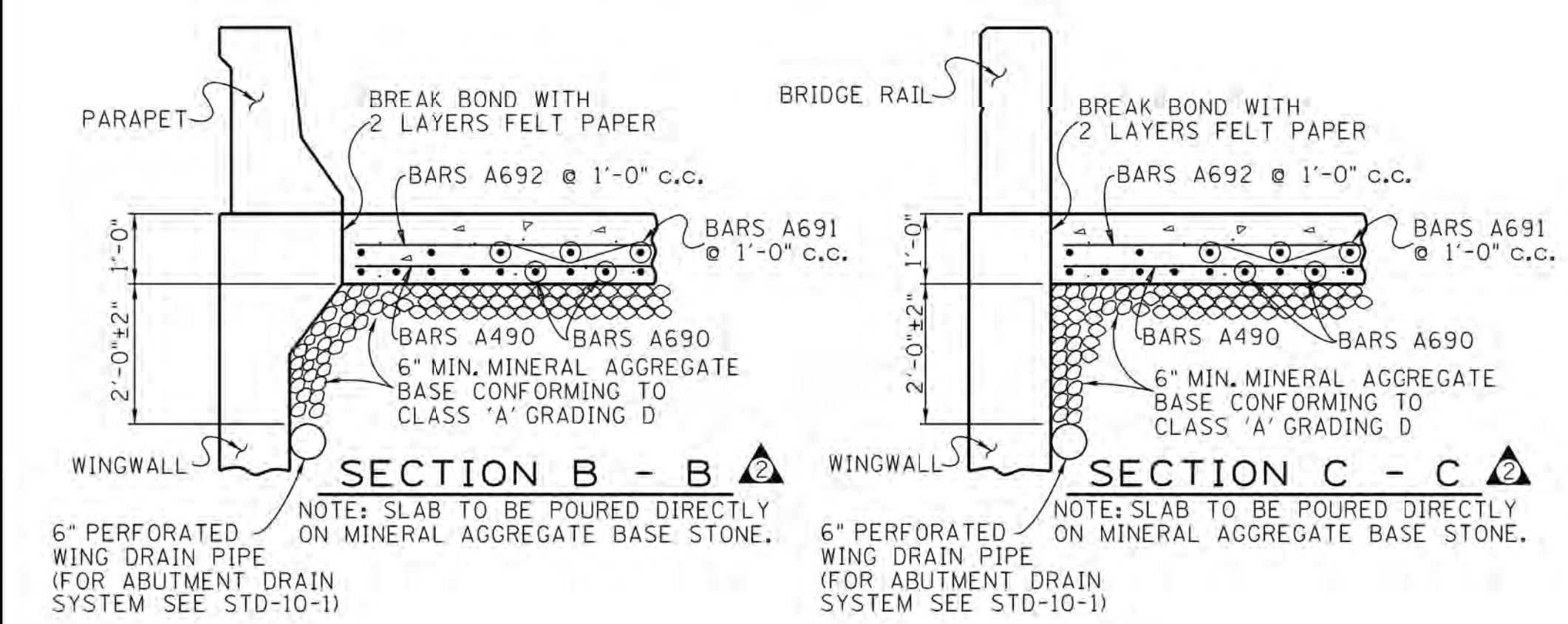
BARS	LOCATION	SIZE	NO. REQ'D	BENDING DIMENSIONS				LENGTH
				A	B	C	D	
B490E	MEDIAN (WHEN REQ'D.)	4	48					2'-8"
A490	SLAB	4	8					△
A690	SLAB	6	△					12'-0"
A691	SLAB	6	△					23'-8"
A692	SLAB	6	31					△
H490	FOOTING	4	△	6"	1'-4"			3'-4"
L490	FOOTING	4	△	2'-7 1/4"	1'-0"	8"		7'-6 1/2"

△ THESE NUMBERS VARY DEPENDING UPON ROADWAY WIDTH.

PROJECT NO.	YEAR	SHEET NO.
	1995	

**REVISIONS**

NO.	DATE	BY	BRIEF DESCRIPTION
1	5-1-95	CMH	GENERAL REVISION REDESIGN
2	12-18-95	CMH	ADDED BARS A490 AND JOINT DETAIL
3	4-28-97	CMH	REVISED JOINT DETAILS AND NOTE
4	9-6-99	CMH	REVISED JOINT NOTE



**NOTES**

- QUANTITIES FOR CLASS 'A' CONCRETE, REGULAR AND EPOXY COATED REINFORCING STEEL (WHEN REQUIRED FOR MEDIAN BARRIER), BACKER ROD, JOINT SEALER, ELASTOMERIC CONCRETE, STYROFOAM, GRATE AND MISCELLANEOUS MATERIALS FOR BRIDGE END DRAIN, WHEN REQUIRED, ARE TO BE INCLUDED IN PAVEMENT AT BRIDGE ENDS, S.Y. FOR BAR BENDING DIMENSIONS SEE THIS SHEET AND BILL OF STEEL FOR BRIDGE END DRAIN ON DRAWING NO. STD-1-6.
- COST OF MINERAL AGGREGATE CLASS A GRADING D BASE QUANTITY SHALL BE INCLUDED IN COST OF PAVEMENT AT BRIDGE ENDS. CLASS B GRADING C OR D MAY ALSO BE USED.

**GENERAL NOTES**

CONCRETE: TO BE CLASS 'A' (f'c = 3,000 psi)

REINFORCING STEEL: SHALL BE ASTM A615 GRADE 60 UNLESS NOTED OTHERWISE, SEE SECTION 604 AND 907 OF THE STANDARD SPECIFICATIONS.

SPECIFICATIONS: STANDARD ROAD AND BRIDGE SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION (CURRENT EDITION).

NOTE: THE APPROACH SLAB SHALL NOT BE POURED UNTIL THE ADJACENT END SPAN DECK SLAB IS IN PLACE AND ACCEPTED BY THE ENGINEER.

NOTE: THE APPROACH SLAB CONTROL ELEVATIONS SHALL BE ADJUSTED, (IF REQUIRED), SO AS TO MATCH THE IN PLACE DECK SLAB IN BOTH TRANSVERSE AND LONGITUDINAL DIRECTIONS.

**JOINT SEAL SYSTEM:** THE EXPANSION JOINT SYSTEM USED SHALL BE ON THE TDOT APPROVED QUALIFIED LISTS FOR ACCEPTABLE PRODUCTS. THE JOINT SYSTEM SHALL BE INSTALLED UNDER THE DIRECT SUPERVISION OF AN AUTHORIZED TECHNICIAN PROVIDED BY THE EXPANSION JOINT SUPPLIER.

MINOR REVISION - FHWA APPROVAL NOT REQUIRED

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

**REINFORCED CONCRETE PAVEMENT AT BRIDGE ENDS 1995**

DESIGNED BY: C.M. HILES  
DRAWN BY: KIM FRANKENFELD  
SUPERVISED BY: C.M. HILES  
CHECKED BY: \_\_\_\_\_

DATE: 4-95  
DATE: 4-95  
DATE: \_\_\_\_\_

**PLAN (90° SKEW)**

**PLAN (VARIABLE SKEW)**

CORRECT *Edward P. Wasserman*  
ENGINEER OF STRUCTURES